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ELECTRONIC DOCUMENTS.

GARVER
4701 NORTHSORE DRIVE
NORTH LITTLE ROCK, AR 72118
AMJAD S. KHAIRI, P.E. NO. 106650

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	STRUCTURE-SIGN1
TITLE SHEET	1
ESTIMATED BRIDGE QUANTITIES.....	2 (BR-131-141)
GENERAL NOTES	3 (BR-131-142)
ENVIRONMENTAL NOTES.....	4 (BR-131-143)
EPSC NOTES.....	4A (BR-131-144) –
.....	4B (BR-131-145)
PROJECT COMMITMENTS	5
LAYOUT OF BRIDGE TO BE REPAIRED	BR-131-140
BRIDGE REPAIR DETAILS	BR-131-146 –
.....	BR-131-147

YEAR	PROJECT NO.	SHEET NO.
2025	43005-4249-04	STRUCTURE-SIGN
<div style="border: 1px solid black; height: 100%; width: 100%;"></div>		
<div style="border: 1px solid black; padding: 5px; text-align: center;"> STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION </div>		
<div style="border: 1px solid black; padding: 20px; width: 80%; margin: 0 auto;"> <p style="font-size: 24px; margin: 0;">SIGNATURE</p> <p style="font-size: 24px; margin: 0;">SHEET</p> </div>		

Index of Sheets

SHEET NO.	REVISION	SHEET NAME
STRUCTURE-SIGN1		SIGNATURE SHEET
1		TITLE SHEET
2		ESTIMATED QUANTITIES
3		GENERAL NOTES
4		ENVIRONMENTAL NOTES
4A		EPSC NOTES
4B		EPSC NOTES
5		PROJECT COMMITMENTS
B1		BRIDGE PLANS
S1-S8		STORM WATER POLLUTION PREVENTION PLAN (SWPPP) PLANS

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

REVISIONS				TENN.	YEAR	SHEET NO
NO.	DATE	BY	BRIEF DESCRIPTION		2025	1
				FED AID PROJ NO		
				STATE PROJ NO	43005-4249-04	

STANDARD ROADWAY AND STRUCTURE DRAWINGS

ROADWAY DESIGN STANDARDS

DWG. NO.	REVISION	DESCRIPTION
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2	-	STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-2	02-20-20	STANDARD LEGEND FOR UTILITY INSTALLATIONS

DESIGN - TRAFFIC CONTROL

DWG. NO.	REVISION	DESCRIPTION
T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-WZ-PBR1	03-26-25	INTERCONNECTED PORTABLE BARRIER RAIL
T-WZ-PBR2	03-26-25	DETAILS FOR WORK ZONE CHANNELIZATION DEVICES
T-WZ-32	03-26-25	TRAFFIC CONTROL PLAN SIGNAL LAYOUT FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-33	03-26-25	TRAFFIC CONTROL PLAN FOR CLOSE INTERSECTION CONDITIONS USING TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-34	03-26-25	TRAFFIC CONTROL PLAN GENERAL NOTES FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-35	03-26-25	TRAFFIC CONTROL PLAN PAY ITEM AND SIGN DETAILS FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE

EROSION PREVENTION AND SEDIMENT CONTROL

DWG. NO.	REVISION	DESCRIPTION
EC-STR-2	08-01-12	SEDIMENT FILTER BAG
EC-STR-3C	03-01-23	SILT FENCE WITH WIRE BACKING
EC-STR-3E	04-01-08	SILT FENCE FABRIC JOINING DETAILS

ROADWAY, PAVEMENT APPURTENANCES, AND FENCES

DWG. NO.	REVISION	DESCRIPTION
S-F-1	03-01-23	HIGH VISIBILITY FENCE

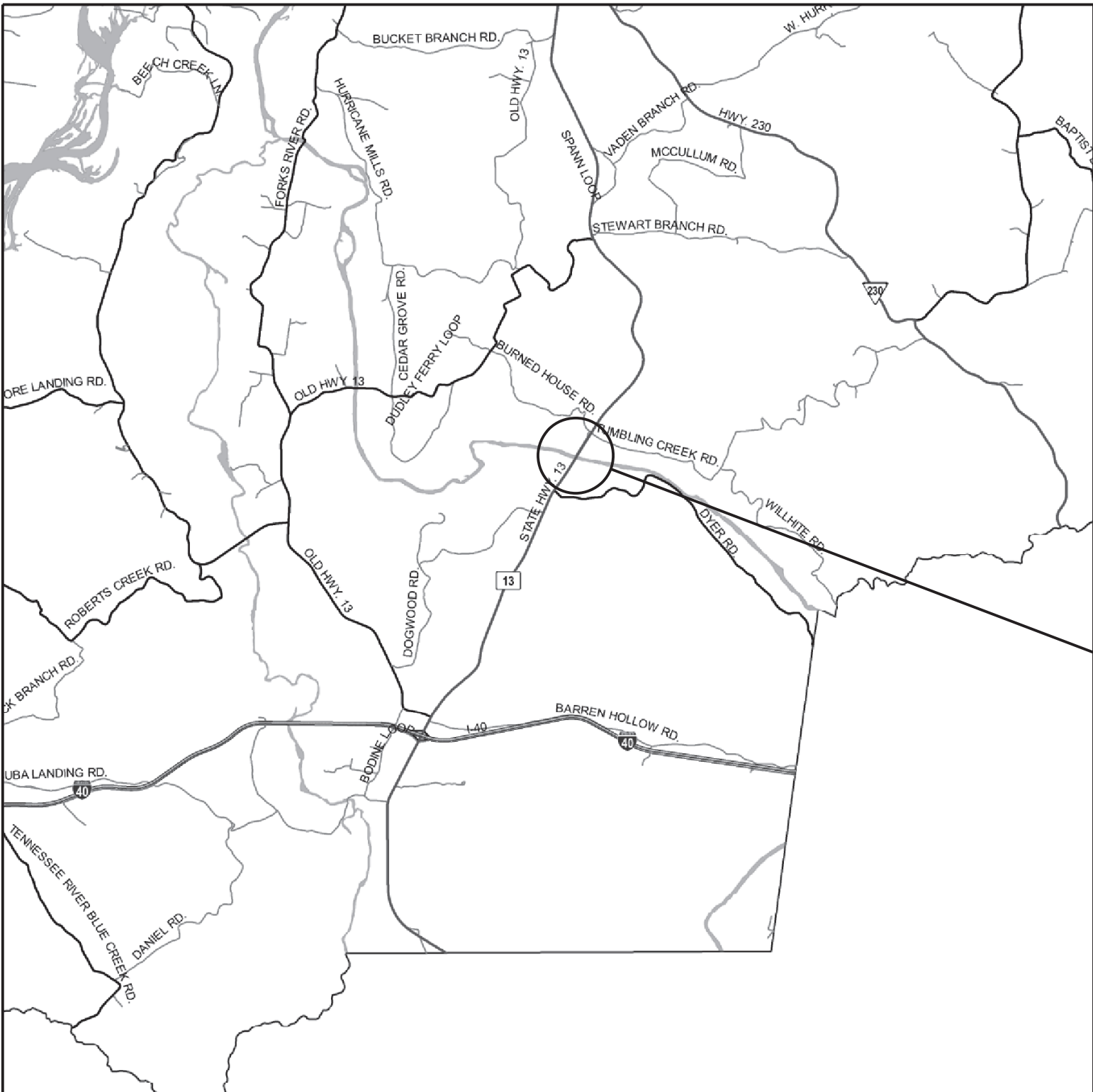
HUMPHREYS COUNTY

SR-13 BRIDGE OVER DUCK RIVER, LM 7.71

PS&E
(BRIDGE REPAIR)

STATE ROUTE 13 F.A.H.S. NO.

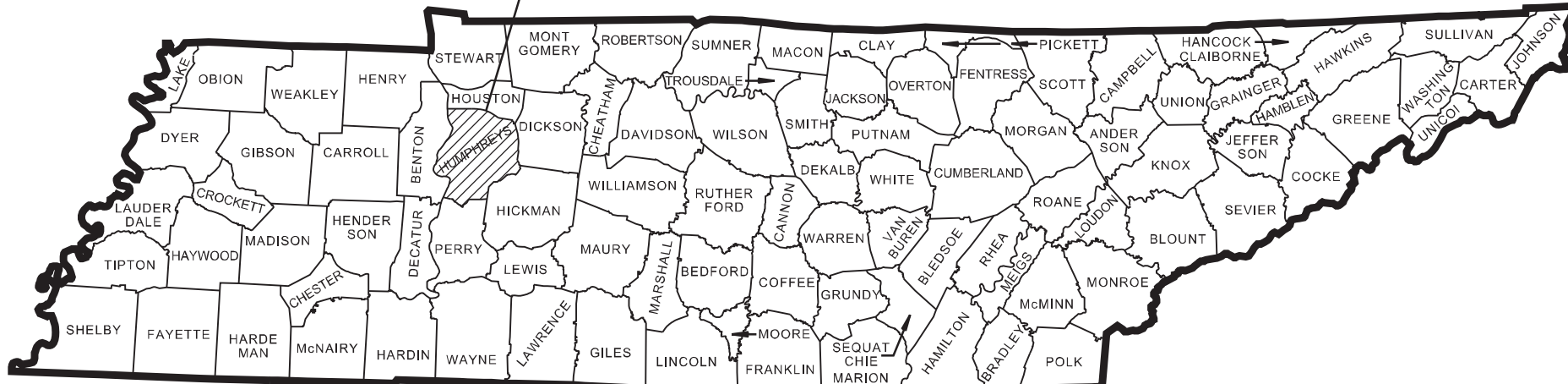
SCALE: NTS



PROJECT LENGTH
0.00 MILE

PROJECT NO. 43005-4249-04
STATE ROUTE 13 - L.M. 7.71

HUMPHREYS COUNTY
BRIDGE I.D. NO. 43SR0130013



DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

VOLUME OF FILL: 220 C.Y.
DISTURBED AREA: 3100 S.F.

CONTRACTOR SHALL OBTAIN THE LATEST UNDERWATER INSPECTION REPORT FROM TDOT.

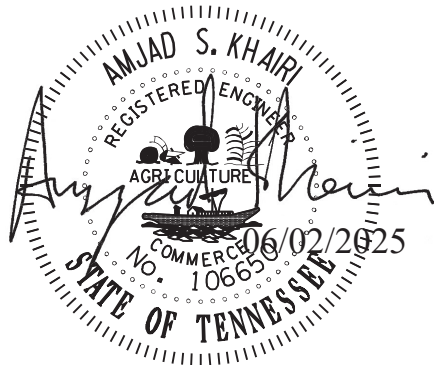
APPROVED: 
WILL REID, DEPUTY COMMISSIONER/
CHIEF ENGINEER

DATE _____

APPROVED: 
HOWARD H. ELEY, DEPUTY GOVERNOR &
COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE _____



SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT REPAIR OFFICE DESIGN MANAGER JOSEPH BENDER
DESIGNED BY GARVER, LLC
DESIGNER A. J. KHAIRI CHECKED BY J. H. RUDDELL
PE NO. 43005-4249-04 PIN. NO. 101560.01

STATE ROUTE 13	
ADT (2023)	2,851
POSTED SPEED	55 MPH

W:\McEntire\6/27/2025 11:44:16 AM
WORKSPACE: TD01 Bridge (2025)
C:\2025\B011428 - TD01 Humphreys Co. SR 13 Duck River Drawings\BRO\08 - Construction - 002 (BR-131-141).dgn

BRIDGE REPAIR QUANTITIES

ITEM	DESCRIPTION	UNIT	TOTAL
⑩ ①	209-08.02 TEMPORARY SILT FENCE (WITH BACKING)	L.F.	2100
	209-09.03 SEDIMENT FILTER BAG (15' X 15')	EACH	1
⑩ ⑤	303-02 MINERAL AGGREGATE, TYPE B BASE, GRADING A	TON	50
⑩	303-10.01 MINERAL AGGREGATE (SIZE 57)	TON	10
②	604-10.15 STRUCTURE REPAIRS (FOUNDATIONS)	LS	1
⑩ ⑨	604-10.50 BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB)	SY	15
⑦	617-04.01 TYPE I THIN EPOXY OVERLAY (EPOXY - URETHANE)	S.Y.	9330
⑩ ⑧	707-08.11 HIGH-VISIBILITY CONSTRUCTION FENCE	L.F.	1500
⑩ ⑥	709-05.06 MACHINED RIP-RAP (CLASS A-1)	TON	50
⑩ ③	709-05.09 MACHINED RIP-RAP (CLASS C)	TON	540
④	712-01 TRAFFIC CONTROL	LS	1
	716-12.02 ENHANCED FLATLINE THERMO PVT MRKNG (6IN LINE)	L.M.	1.6
	717-01 MOBILIZATION	LS	1
⑩	740-10.03 GEOTEXTILE (TYPE III) EROSION CONTROL	S.Y.	100
⑩	801-03 WATER (SEEDING & SODDING)	M.G.	3
⑩	803-01 SODDING (NEW SOD)	S.Y.	300

- ① INCLUDES THE COST OF ALL LABOR AND MATERIALS FOR FURNISHING AND INSTALLING THE TEMPORARY SILT FENCE WHERE LOCATED BY THE ENGINEER, AND REMOVAL UPON PROJECT COMPLETION. SEE STD. EC-STR-3C AND EC-STR-3E FOR NOTES AND DETAILS. SEE SUBSECTION 209.07 OF THE SPECIFICATIONS FOR INSTALLATION AND MAINTENANCE INSTRUCTIONS. SEE DWG. NO. BR-131-140 FOR LOCATIONS.
- ② INCLUDES ALL COSTS OF LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO PROVIDE REPAIRS AT PIER FOOTING. THIS ITEM INCLUDES TRIMIE CONCRETE AND FORMING. FOR LOCATION, SEE DWG. NO. BR-131-140. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-130-146. THIS ITEM SHALL ALSO INCLUDE ALL COST OF PROVIDING A COMMERCIAL CERTIFIED DIVER TO HELP WITH THE INSTALLATION IF REQUIRED BY CONTRACTOR MEANS AND METHODS. THIS ITEM WILL ALSO INCLUDE COSTS OF ALL LABOR, MATERIALS, EQUIPMENT TO CLEAR AND HAUL ALL DEBRIS AROUND PIER NOS. 1 AND 2 SHALL BE INCLUDED IN THIS ITEM. ALL WORK SHALL MEET THE APPROVAL OF THE TDOT ENGINEER.
- ③ THIS ITEM INCLUDES ALL COSTS OF LABOR AND MATERIALS TO HAUL AND PLACE CLASS C RIPRAP AT BRIDGE NO. 43-SR013-7.71 AT PIER NOS. 1 AND 2 AS DIRECTED BY THE ENGINEER FROM TDOT. FOR NOTES AND DETAILS, SEE DWG. NO. BR-131-146.
- ④ THIS ITEM SHALL INCLUDE COST OF PROVIDING ANY SIGNING AND TRAFFIC CONTROL APPURTENANCES FOR PAVING OPERATIONS AND OTHER REPAIRS SHOWN ON THESE PLANS AND INCLUDING FOUNDATION REPAIR AT PIER NOS. 1 AND 2, DEBRIS REMOVAL, EPOXY OVERLAY AND PARTIAL DEPTH DECK REPAIR.
- ⑤ THIS ITEM INCLUDES ALL COSTS OF LABOR AND MATERIALS NECESSARY TO REPAIR THE EXISTING HAUL ROAD USING MINERAL AGGREGATE, TYPE B BASE, GRADING A AT BRIDGE NO. 43-SR013-7.71. SEE DWG. NO. BR-131-140 FOR HAUL ROAD LOCATION.
- ⑥ INCLUDES THE COSTS OF ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY TO REPAIR THE EXISTING HAUL ROAD USING RIP-RAP (CLASS A-1) AT BRIDGE NO. 43-SR013-7.71. SEE DWG. NO. BR-131-140 FOR HAUL ROAD LOCATION.
- ⑦ INCLUDES ALL COSTS FOR LABOR AND MATERIALS TO PLACE TYPE 1 THIN EPOXY OVERLAY FOR THE ENTIRE LENGTH AND WIDTH OF BRIDGE. SEE NOTES THIS DRAWING FOR ADDITIONAL DETAILS.
- ⑧ INCLUDES THE COST OF ALL LABOR AND MATERIALS TO PLACE HIGH VISIBILITY FENCE AROUND BOTH WETLAND AREAS, WTL-1 AND WTL-2.
- ⑨ INCLUDES ALL COSTS TO PERFORM PARTIAL DEPTH DECK REPAIRS. SEE DECK REPAIR NOTES ON DWG. NO. BR-131-150A.
- ⑩ THE ENGINEER MAY INCREASE OR DECREASE THE QUANTITY FOR THIS ITEM.

THIN EPOXY OVERLAY NOTES
(ITEM NO. 617-04.01)

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD:

A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.

B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.

C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.

D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.

E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.

F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 14 DAYS BEFORE THE OVERLAY IS PLACED. THE 14 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS. THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACK COAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THE THIN EPOXY OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACK COAT OR DEBRIS REMOVAL. REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), S.Y.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, ETC., BUT IN ALL CASES, ANY DESTRUCTIVELY TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

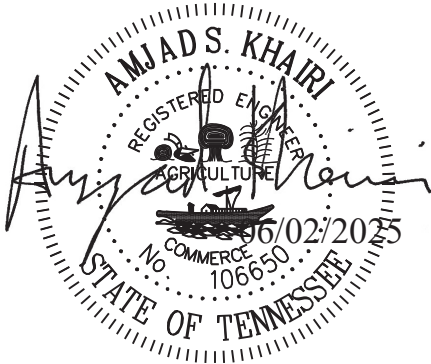
PROJECT NO.		YEAR	SHEET NO.
43005-4249-04		2025	2
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED QUANTITIES

SR13 OVER DUCK RIVER
BRIDGE I.D. NO. 43SR0130013
BRIDGE NO. 43-SR013-7.71

HUMPHREYS COUNTY
2025



DESIGNED BY D. P. ERICKSON DATE DEC. 2020
DRAWN BY C. W. THOMAS DATE DEC. 2020
SUPERVISED BY J. H. RUDELL DATE DEC. 2020
CHECKED BY A. J. KHAIRI DATE DEC. 2020

TN D.O.T. ENGINEERING SUPERVISOR J. BENDER

\\NWCEntire 6/27/2025 11:44:16 AM
WORKSPACE: T001 Br099 (202)
C:\2025\B011428 - T001 Humphreys Co. SR 13 Duck River Drawings\BRC\08 - Certified (2025 Letting)\01 - Construction - 003 (BR-131-142).dgn

GENERAL NOTES

SPECIFICATIONS AND LOADING

- ① SPECIFICATIONS:
CONSTRUCTION SPECIFICATIONS:STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (JANUARY 1,2021 EDITION), AND THE 4" EDITION (2017) AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS WITH INTERIMS.
- ② DESIGN SPECIFICATIONS:
9TH EDITION (2020) AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS WITH INTERIMS, AND THE 2ND EDITION (2011) AASHTO GUIDE SPECIFICATIONS FOR LRFD SEISMIC BRIDGE DESIGN WITH INTERIMS.

CONCRETE

- ③ TREMIE CONCRETE:
SHALL BE CLASS "A" CONCRETE AND SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF SUBSECTION 604.18 OF THE STANDARD SPECIFICATIONS FOR DEPOSITING CONCRETE UNDERWATER EXCEPT THE CEMENT CONTENT OF THE CONCRETE SHALL CONTAIN A MINIMUM OF 682 POUNDS PER CUBIC YARD. ALSO, A TYPE D-WATER REDUCING AND RETARDING ADMIXTURE, MEETING THE REQUIREMENTS OF ASTM C494-2019 SHALL BE ADDED TO THE MIX IN THE PROPORTIONS RECOMMENDED BY THE MANUFACTURER OF THE TREMIE CONCRETE AND AS APPROVED BY THE ENGINEER. THE SLUMP OF THE CONCRETE SHALL BE MAINTAINED BETWEEN 4 INCHES AND 8 INCHES AS DIRECTED BY THE ENGINEER. THE SEAL SHALL BE PLACED THROUGH TREMIE PIPES AS SPECIFIED IN THE STANDARD SPECIFICATIONS EXCEPT THAT THE LOWER OR DISCHARGE END OF THE TREMIE SHALL BE EQUIPPED WITH A SUITABLE VALVE OR DEVICE WHICH SHALL BE TIGHTLY CLOSED WHILE THE TREMIE IS BEING CHARGED AND LOWERED INTO POSITION AND WHICH CAN BE FULLY OPENED IN THE LOWERED POSITION.

STEEL

- ④ STRUCTURAL STEEL:
SHALL BE AASHTO M270 GRADE 50 STEEL.
- ⑤ BOLTS:
SHALL BE HIGH TENSILE STRENGTH BOLTS (ASTM-F3125, GRADE A325 OR A490), TYPE 1, UNLESS OTHERWISE NOTED. BOLT SIZE TO BE AS NOTED ON PLANS. SEE SECTION 602 OF THE STANDARD SPECIFICATIONS. EXISTING CONTACT SURFACES SHALL BE CLEANED OF ALL EXISTING PAINT AND RUST TO BARE METAL PRIOR TO ATTACHMENT OF NEW MEMBERS. UNCOATED (BLACK) BOLTS MAY BE USED INSTEAD OF GALVANIZED BOLTS.

STEEL REPAIR

- ⑥ WELDING:
SEE CURRENT EDITION OF THE AASHTO/AWS D1.5 BRIDGE WELDING CODE. CONTRACTOR IS TO SUBMIT WELDING PROCEDURE SPECIFICATIONS (BASED ON SUCCESSFUL TEST RESULTS AS RECORDED IN A PROCEDURE QUALIFICATION TEST RECORD, SEE AASHTO/AWS D1.5 SECTION 1.9 AND SECTION 7 (QUALIFICATION) AND WELDER CERTIFICATIONS TO THE ENGINEER FOR REVIEW BEFORE WELDING WILL BE ALLOWED. WELDER CERTIFICATION SHALL INCLUDE PROOF OF CONTINUOUS WORK USING THE SPECIFIED WELDING PROCESS.

THE WELDING PROCEDURE SPECIFICATIONS ARE NOT REQUIRED FOR TEMPORARY WELDS; (STAY-IN-PLACE METAL DECKING IS CONSIDERED TEMPORARY.)

THE WELDING PROCEDURE SPECIFICATIONS SHOULD BE ON SITE FOR WELDER AND INSPECTOR REVIEW.

WELDING IS TO BE DONE BY QUALIFIED WELDERS. SUPERVISION OF NON-QUALIFIED WELDERS IS NOT PERMITTED. T00T HAS THE OPTION OF HAVING THE WELDER RECERTIFIED IF QUESTIONABLE WORK IS OBSERVED; THE COST OF THIS REQUALIFICATION IS TO BE PAID FOR BY THE CONTRACTOR.

WELDER QUALIFICATION POSITIONS FOR FILLET AND GROOVE WELDS: FLAT (F), HORIZONTAL (H), VERTICAL (V), OVERHEAD (OH). SEE FIG 7.4 OR FIG 7.5 IN AASHTO/AWS D1.5 FOR POSITION LIMITS.

QUALIFICATION TEST		TYPE OF WELD & POSITION OF WELDING QUALIFIED	
WELD		PLATE	
POSITION		GROOVE	FILLET
GROOVE	1G	F	F, H
	2G	F, H	F, H
	3G	F, H, V	F, H, V
	4G	F, OH	F, H, OH
3G & 4G		ALL	ALL
FILLET	1F		F
	2F		F, H
	3F		F, H, V
	4F		F, H, OH
3F & 4F			ALL

FROM TABLE 7.10, WELDER QUALIFICATION - TYPE AND POSITION LIMITATIONS, AASHTO/AWS D1.5

MISCELLANEOUS GENERAL NOTES

- ⑦ SHOP DRAWINGS:
REFER TO SECTION 105.02 OF THE STANDARD SPECIFICATIONS. IF USING PAPER COPIES, SHOP DRAWINGS ARE TO BE SENT TO THE BRIDGE REPAIR OFFICE IN THE DIVISION OF STRUCTURES, FOR ELECTRONIC SUBMITTALS, SEE SECTION 105.02. EACH SHOP DRAWING SHALL CONTAIN IN THE TITLE BLOCK THE FOLLOWING: THE STATE, PROJECT NUMBER, COUNTY, BRIDGE NAME, BRIDGE NUMBER (OR STRUCTURE TYPE AND NUMBER), STATION, AND CONTRACT NUMBER. SHOP DRAWINGS WITH TITLE BLOCKS NOT INCLUDING THE FOREGOING IDENTIFICATION WILL BE RETURNED FOR CORRECTION BEFORE ANY REVIEWS FOR APPROVAL ARE CONDUCTED.
- ⑧ SPECIAL NOTE TO CONTRACTOR:
CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURES NECESSARY TO INSURE THAT NO DEBRIS IS DROPPED INTO THE STREAM. ANY DEBRIS WHICH IS ALLOWED TO DROP ON THE BANKS BELOW THE BRIDGE SHALL NOT BE ALLOWED TO ENTER THE STREAM AND SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. COST OF REMOVAL AND DISPOSAL OF DEBRIS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR OTHER ITEMS.
- ⑨ NON-PAY ITEMS:
ONLY ITEMS SHOWN ON THE PROPOSAL AS PAY ITEMS WILL BE PAID FOR. COMPENSATION FOR ALL LABOR, MATERIALS, TOOLS, EQUIPMENT, AND INCIDENTALS FOR THE ENTIRE CONTRACT SHALL BE INCLUDED IN THE PRICE FOR THE PAY ITEMS



DESIGNED BY D. P. ERICKSON
DRAWN BY C. W. THOMAS
SUPERVISED BY J. H. RUDELL
CHECKED BY A. J. KHAIRI

DATE DEC. 2020
DATE DEC. 2020
DATE DEC. 2020
DATE DEC. 2020

TN D.O.T. ENGINEERING SUPERVISOR J. BENDER

CONST. WORK ZONE AND TRAFFIC CONTROL

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE (1) WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQUARE FOOT.

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DEIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL DETOURS SHALL BE PAVED, STRIPED, SIGNED AND FLEXIBLE DRUMS ARE TO BE IN PLACE BEFORE IT IS OPENED TO TRAFFIC.

THE CONTRACTOR SHALL COMPLY FULLY WITH ANY REQUIREMENTS ESTABLISHED BY THE CORPS OF ENGINEERS, U.S. COAST GUARD, AND ANY OTHER AGENCIES WHICH MAY HAVE JURISDICTION RELATIVE TO CONSTRUCTION WORK OVER A NAVIGABLE STREAM WHICH IS APPLICABLE TO THIS CONTRACT, AND WHICH MAY NOT BE COVERED BY EXISTING PERMITS. THE CONTRACTOR SHALL ALSO NOTIFY THE CORPS OF ENGINEERS INFORMING THEM OF WORK TO BE PERFORMED BEFORE ANY WORK OVER THE WATERWAY BEGINS. THE CONTRACTOR SHALL SUBMIT A DESCRIPTION OF WORK AND SKETCHES OF ANY FALSEWORK, SCAFFOLDING, DEBRIS CONTAINMENT SYSTEMS, ETC. WHICH MAY BE REQUIRED DURING CONSTRUCTION WHICH MAY ENCROACH UPON THE VERTICAL AND/OR HORIZONTAL CLEARANCES FOR WATERWAY TRAFFIC TO THE U.S. COAST GUARD FOR APPROVAL BEFORE ANY WORK IS BEGUN.

CONTACT: ERIC WASHBURN
U.S. COAST GUARD
BRIDGE ADMINISTRATOR
1222 SPRUCE STREET
ST. LOUIS, MO 63103-2832

UTILITY NOTES

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT PER "THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT" THAT ANYONE WHO ENGAGES IN EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOTE MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106, WILL BE REQUIRED.

UNLESS OTHERWISE NOTIFIED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCED CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF UTILITIES. PRIOR TO COMMENCING THE WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC. AT 1-800-351-1111 WILL BE REQUIRED.

PROJECT NO.		YEAR	SHEET NO.
43005-4249-04		2025	3
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

MISCELLANEOUS

NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS/HER RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT DATA AND UNDERWATER INSPECTION REPORT. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY CONSTRUCTION.

ALL DIMENSIONS OF THE EXISTING COMPONENTS SHOWN ON THE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY ALL DATA NECESSARY TO CONNECT THE NEW MATERIAL AND SHALL BE SOLELY RESPONSIBLE FOR THE ACCURACY THEREOF.

BIDDERS SHALL FULLY INFORM THEMSELVES OF THE NATURE OF THE WORK AND CONDITIONS UNDER WHICH IT WILL BE PERFORMED. THE CONTRACTOR SHALL ADOPT METHODS CONSISTENT WITH GOOD CONSTRUCTION PRACTICE AND SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT DAMAGE TO THE EXISTING BRIDGE OR ATTACHMENTS. ANY DAMAGE TO THE EXISTING BRIDGE STRUCTURE OR ROADWAY DUE TO THE CONTRACTOR'S NEGLIGENCE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE, TO THE SATISFACTION OF THE ENGINEER. ALL COSTS ASSOCIATED WITH THE DIFFICULT NATURE OF THE CONSTRUCTION SITE INCLUDING SPECIALLY CRANE, LIFTS, OR HOISTS SHALL BE INCLUDED IN THE COST OF ITEMS BID ON.

THE CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURE NECESSARY THAT NO DEBRIS IS DROPPED INTO THE STREAM AT BRIDGE AT LOG MILE 7.71. THIS SHALL BE ACCOMPLISHED BY THE USE OF BASKETS, NETTING, WRAPPING, WORK PLATFORM, OR OTHER SIMILARLY EFFECTIVE MEANS. ANY DEBRIS WHICH IS ALLOWED TO DROP ON THE BANKS BELOW THE BRIDGE SHALL NOT BE ALLOWED TO ENTER THE STREAM AND SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. COST OF REMOVING AND DISPOSING OF DEBRIS SHALL BE INCLUDED IN ITEMS BID ON.

THE CONTRACTOR SHALL NOT DISTURB ANY PORTION OF THE STREAM AT LOG MILE 7.71.

THE CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURE NECESSARY TO ENSURE THAT STREAM "STR-1" AT THE BRIDGE WILL NOT BE DISTURBED. THE ENTIRE AREA UNDER THE BRIDGE OUTSIDE THE WORK AREA SHALL NOT BE DISTURBED.

STEEL SHEET PILING

THE CONTRACTOR SHALL DRIVE NEW P222 STEEL SHEET PILING TO PROVIDE COFFERDAM AT PIER NO. 1. SHEET PILING SHALL BE AASHTO M270 GRADE 50 STEEL, SEE DWG. NOS. BR-131-145 THRU BR-131-148 AND BR-131-150 FOR ADDITIONAL DETAILS. ALL WORK MUST MEET FULL APPROVAL OF THE ENGINEER.

FINAL PAVEMENT MARKING - ITEM NO. 716-12.02 (CENTERLINE AND EDGELINE)

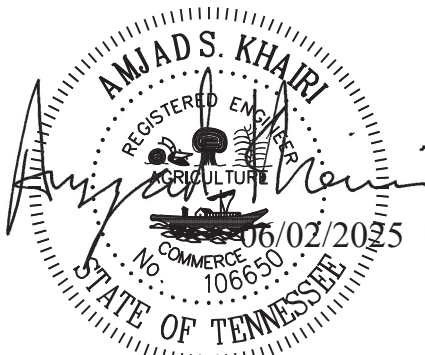
PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAYS WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PAVEMENT MARKING (6" LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

SR13 OVER DUCK RIVER
BRIDGE I.D. NO. 43SR0130013
BRIDGE NO. 43-SR013-7.71

HUMPHREYS COUNTY
2025



6/27/2025 11:44:16 AM
WORKSPACE: TDOT Bridge (2025)
C:\2025\B011428 - TDOT Humphreys Co. SR 13 Duck River Drawings\BRC\08 - Certified (2025 Letting)\01 - Construction - 004 (BR-131-143).dgn

ENVIRONMENTAL NOTES

SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT NATURAL RESOURCES AND WATER QUALITY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG NATURAL RESOURCES IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS, WETLANDS, OR OTHER NATURAL FEATURES IN ACCORDANCE WITH TDOT STANDARDS. EPSC MEASURES SHALL BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.

NEW CHANNEL CONSTRUCTION SHALL BE COMPLETED IN THE DRY AND STABILIZED FOR AT LEAST 72 HOURS PRIOR TO DIVERTING WATER FROM THE EXISTING AND/OR TEMPORARY CHANNEL.

INSTREAM EPSC DEVICES REQUIRE THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN WATER QUALITY PERMITS. THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.

THE WIDTH OF THE FILL ASSOCIATED WITH TEMPORARY CROSSINGS SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR THE ACTUAL CROSSING, NOT TO EXCEED THE WIDTH SPECIFIED IN THE STANDARD DRAWING.

STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CULVERT CROSSINGS SHALL BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES SHALL BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAM BED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK SHALL BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS SHALL BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO PREEXISTING ELEVATIONS. ALL TEMPORARY CROSSINGS SHALL BE CONSTRUCTED IN TDOT ROADWAY DESIGN GUIDELINES ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (E.G. BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAM BED IS AN ACCEPTABLE OPTION.

HEAVY EQUIPMENT WORKING IN WETLANDS WITH PERMITTED TEMPORARY IMPACTS SHALL BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE AND COMPACTION UNLESS SPECIFICALLY ADDRESSED IN THE PLANS, SPECIFICATION, AND ESTIMATE (PS&E) PLANS. ANY MATS AND OTHER MEASURES USED FOR HEAVY EQUIPMENT SHALL BE REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED. ALL AFFECTED AREAS SHOULD BE RETURNED TO PRE-EXISTING CONDITIONS.

WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS, UNLESS SPECIFICALLY PROVIDED FOR IN THE PLANS, SPECIFICATION, AND ESTIMATE (PS&E) PLANS AND PERMITS.

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

NO ACTIVITY MAY SUBSTANTIALLY DISRUPT THE MOVEMENT OF THOSE SPECIES OF AQUATIC LIFE INDIGENOUS TO THE WATER BODY, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA.

SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).

IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO ARCHAEOLOGY, ECOLOGY, HISTORICAL, HAZARDOUS MATERIALS, AIR AND NOISE, TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING ANY MATERIAL AND STAGING AREAS AND THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS TO BE USED. ANY SUCH PERMITS SHALL BE SUPPLIED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO THE USE OF THE PERMITTED AREA(S).

ANY DISAGREEMENT BETWEEN THE PLANS, SPECIFICATION, AND ESTIMATE (PS&E) PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT RESPONSIBLE PARTY. THE ENVIRONMENTAL DIVISION, DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.

IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATE. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.

ALL WATER QUALITY PERMITS SHALL BE POSTED NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE ACCESSIBLE TO THE PUBLIC. THE NAME, COMPANY NAME, EMAIL ADDRESS, TELEPHONE NUMBER AND ADDRESS OF THE PROJECT SITE OWNER, OPERATOR, OR A LOCAL CONTACT PERSON WITH A BRIEF DESCRIPTION OF THE PROJECT SHALL ALSO BE POSTED. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS INFEASIBLE, THE INFORMATION SHALL BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION NEAR WHERE THE CONSTRUCTION IS ACTIVELY UNDERWAY AND MOVED AS NECESSARY. THIS LOCATION SHALL BE POSTED AT THE CONSTRUCTION SITE. ALL POSTINGS SHALL BE MAINTAINED IN LEGIBLE CONDITION.

MATERIALS AND STAGING AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY ENVIRONMENTAL PERMITS, OBTAINED SOLELY BY THE CONTRACTOR. THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATES. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.

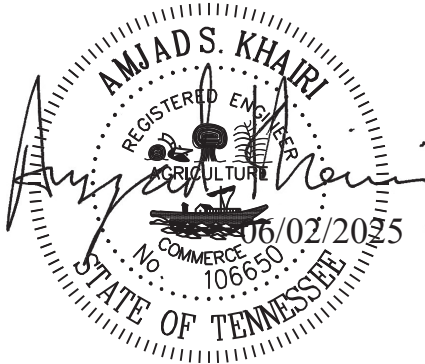
PROJECT NO.		YEAR	SHEET NO.
43005-4249-04		2025	4
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL NOTES

SR13 OVER DUCK RIVER
BRIDGE I.D. NO. 43SR0130013
BRIDGE NO. 43-SR013-7.71

HUMPHREYS COUNTY
2025



DESIGNED BY M. A. SPRADLIN DATE DEC. 2020
DRAWN BY M. A. SPRADLIN DATE DEC. 2020
SUPERVISED BY J. H. RUDELL DATE DEC. 2020
CHECKED BY A. J. KHAIRI DATE DEC. 2020

TN D.O.T. ENGINEERING SUPERVISOR J. BENDER

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WORKSPACE: TDOT Bridge (202)

C:\2025\B011428 - TDOT Humphreys Co. SR 13 Duck River Drawings\BRC\08 - Certified (2025 Letting)\01 - Construction - 0044 - BR-131-144.dgn



DESIGNED BY M. A. SPRADLIN DATE DEC. 2020
DRAWN BY M. A. SPRADLIN DATE DEC. 2020
SUPERVISED BY J. H. RUDELL DATE DEC. 2020
CHECKED BY A. J. KHAIRI DATE DEC. 2020

TN D.O.T. ENGINEERING SUPERVISOR J. BENDER

SUBSECTION 3 - EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

9-410.00 EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

9-410.01 DISTURBED AREA

IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.

UNLESS OTHERWISE NOTED IN THE PLANS, THE CONTRACTOR SHALL NOT CLEAR/DISTURB ANY AREA BEYOND 15 FEET FROM SLOPE LINES.

PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 14 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS APPLIED.

CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION, INCLUDING STREAM AND WETLAND BUFFERS (UNLESS PERMITTED), SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.

9-410.02 SEDIMENT CONTROL

EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.

TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY BUT MUST BE REINSTALLED AT THE END OF THE WORKDAY OR BEFORE/DURING A PRECIPITATION EVENT.

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.) INTO WATERS OF THE STATE (U.S. OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION EXIT (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.

THE DEWATERING OF WORK AREAS, TRENCHES, FOUNDATIONS, EXCAVATIONS, ETC. THAT HAVE COLLECTED STORMWATER, WATER FROM VEHICLE WASH AREAS, OR GROUNDWATER SHALL BE EITHER HELD IN SETTLING BASINS OR TREATED BY FILTRATION AND/OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER DISCHARGED SHALL NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITHIN THE RECEIVING NATURAL RESOURCE. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL-VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.

9-410.03 INSPECTION, MAINTENANCE, & REPAIR

THE TDOT CONSTRUCTION SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S RESPONSIBLE PARTY ARE RESPONSIBLE FOR INSPECTIONS, MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR, THE TDOT CONSTRUCTION SUPERVISOR OR THEIR DESIGNEE SHALL COMPLETE THE EPSC INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.

TDOT CONSULTANTS AND CONTRACTOR STAFF RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. TDOT STAFF AND SUPERVISORS RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDOT "FUNDAMENTALS OF EROSION AND SEDIMENT CONTROL" CLASS AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION.

EPSC CONTROLS SHALL BE INSPECTED ACCORDING TO PERMIT REQUIREMENTS TO VERIFY MEASURES HAVE BEEN INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS, SPECIFICATIONS, AND GOOD ENGINEERING PRACTICES. EPSC INSPECTIONS SHALL BE DOCUMENTED ON THE TDOT EPSC INSPECTION REPORT.

DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING EROSION AND CONTROLLING SEDIMENT INCLUDING SIGNIFICANT IMPACTS TO SURROUNDING NATURAL RESOURCES AND ADJACENT PROPERTY OWNERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWN GRADIENT LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFFSITE ROADWAY SEDIMENT TRACKING.

UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE 24 HOUR TIMEFRAME, WRITTEN DOCUMENTATION SHALL BE PROVIDED IN THE FIELD DIARY AND EPSC INSPECTION REPORT. AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.

INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES SHALL BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL THE CONTRACTOR SHALL TAKE STEPS TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE EPSC MEASURES AT THE CONTRACTOR'S OWN EXPENSE.

THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.

SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE ONTO ADJACENT PROPERTIES AND INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.

9-410.04 EROSION PREVENTION

CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.

THE ACCEPTED EPSC PLAN SHALL REQUIRE THAT EPSC MEASURES BE IN PLACE BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CULVERT OR BRIDGE CONSTRUCTION, CUTTING, FILLING, OR ANY OTHER EARTHWORK OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.

NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE TDOT RESPONSIBLE PARTY. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE BASIC EPSC DEVICES ON THE EPSC PLAN.

TEMPORARY STABILIZATION SHALL BE INITIATED WITHIN 14 DAYS (2 WEEKS) WHEN CONSTRUCTION ACTIVITIES ON A PORTION OF THE SITE ARE TEMPORARILY CEASED AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME UNTIL AFTER 14 DAYS (2 WEEKS). PERMANENT STABILIZATION MEASURES IN DISTURBED AREAS SHALL BE INITIATED WITHIN 14 DAYS (2 WEEKS) AFTER FINAL GRADING OF ANY PHASE OF CONSTRUCTION.

STEEP SLOPES SHALL BE TEMPORARILY STABILIZED NOT LATER THAN 7 DAYS (A WEEK) AFTER CONSTRUCTION ACTIVITY ON THE SLOPE HAS TEMPORARILY OR PERMANENTLY CEASED. STEEP SLOPES ARE DEFINED AS A NATURAL OR CREATED SLOPE OF 35% GRADE OR GREATER REGARDLESS OF HEIGHT.

PERMANENT STABILIZATION WILL REPLACE TEMPORARY MEASURES AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.

TEMPORARY OR PERMANENT STABILIZATION MUST BE FREE OF FINES (SILT AND CLAY SIZED PARTICLES). UNPACKED GRAVEL CONTAINING FINES OR CRUSHER-RUN WILL NOT BE CONSIDERED SUFFICIENT STABILIZATION.

DELAYING THE PLANTING OF COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED.

9-410.05 PERMITS, PLANS & RECORDS

THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER A CHANGE IN THE DESIGN OR CONSTRUCTION OF THE PROJECT OCCURS. THE STAGES DEPICTED IN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL PHASES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION, THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS PHASES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE PHASES OF CONSTRUCTION THAT WILL OCCUR; THUS THESE DOCUMENTS WILL HAVE TO BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.

9-410.06 GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.

CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.

WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.

IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.

ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.

ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.

ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.

OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.

DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.

WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

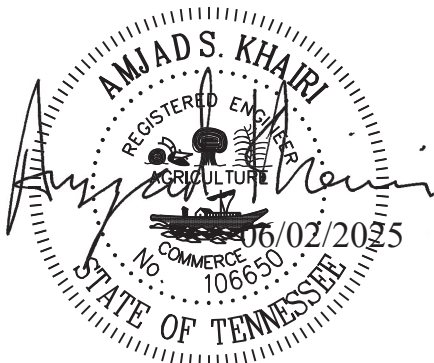
PROJECT NO.		YEAR	SHEET NO.
43005-4249-04		2025	4A
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EPSC NOTES

SR13 OVER DUCK RIVER
BRIDGE I.D. NO. 43SR0130013
BRIDGE NO. 43-SR013-7.71

HUMPHREYS COUNTY
2025



\\WMCEnt\fr 6/27/2025 11:44:17 AM
WORKSPACE: TDOT Bridge (2025)
C:\2025\B011428 - TDOT Humphreys Co. SR 13 Duck River Drawings\BRC\08 - Certified (2025 Letting)\01 - Construction - 004B (BR-131-145).dgn

SUBSECTION 3 - EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES (CONT.)

9-410.07 SUPPORT ACTIVITIES

IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.

MATERIALS AND STAGING AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN.

IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY EPSC PLANS FOR THE MATERIAL AND STAGING AREAS TO THE ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE FOR REVIEW.

9-410.08 SPILL PREVENTION, MANAGEMENT & NOTIFICATION

ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.

FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.

APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.

ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.

THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.

IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.

FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.

IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNRI00000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.

CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

9-410.09 STREAMS, WETLANDS & BUFFER ZONES

ANY WORK WITHIN THE STREAM CHANNEL AREA (E.G., PIER FOOTING, RIP-RAP PLACEMENT, CULVERT/BRIDGE CONSTRUCTION, ETC.) SHALL BE SEPARATED FROM FLOWING WATER OR EXPECTED FLOW PATH AND PERFORMED DURING LOW FLOW CONDITIONS. ALL ITEMS USED WITHIN THE STREAM CHANNEL AREA FOR DIVERSION OF FLOW (OR EXPECTED FLOW), UNLESS SPECIFIED IN THE PLANS, SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE COST OF OTHER ITEMS. THIS NOTE EXCLUDES ANY ITEMS SPECIFIED IN THE PLANS FOR THE TEMPORARY DIVERSION CHANNELS (EC-STR-31) AND TEMPORARY DIVERSION CULVERTS (EC STR-32) FOR SINGLE BARREL CULVERT CONSTRUCTION.

SUBSECTION 4 - EROSION PREVENTION AND SEDIMENT CONTROL SPECIAL NOTES

9-415.00 EROSION PREVENTION AND SEDIMENT CONTROL SPECIAL NOTES

9-415.01 STREAMS, WETLANDS & BUFFER ZONES

FOR PROJECTS THAT DISCHARGE INTO KNOWN EXCEPTIONAL TENNESSEE WATERS OR WATERS IMPAIRED BY SILTATION, A 60 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM WITH THIS DESIGNATION SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 60 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 30 FEET AT ANY MEASURED LOCATION.

A 30 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 30 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 15 FEET AT ANY MEASURED LOCATION. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES.

BUFFER ZONES ARE NOT SEDIMENT CONTROL MEASURES AND MUST NOT BE RELIED UPON AS PRIMARY SEDIMENT CONTROL MEASURES. THE RIPARIAN BUFFER ZONE SHALL BE ESTABLISHED BETWEEN THE TOP OF THE STREAM BANK AND THE DISTURBED CONSTRUCTION AREA. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES. BEST MANAGEMENT PRACTICES (BMPs) PROVIDING EQUIVALENT PROTECTION AS THE NATURAL RIPARIAN ZONE MAY BE USED. WHERE ISSUED, ARAP/401 REQUIREMENTS WILL PREVAIL IF IN CONFLICT WITH THESE BUFFER ZONE REQUIREMENTS.



DESIGNED BY M. A. SPRADLIN DATE DEC. 2020
DRAWN BY M. A. SPRADLIN DATE DEC. 2020
SUPERVISED BY J. H. RUDDELL DATE DEC. 2020
CHECKED BY A. J. KHAIRI DATE DEC. 2020

TN D.O.T. ENGINEERING SUPERVISOR J. BENDER

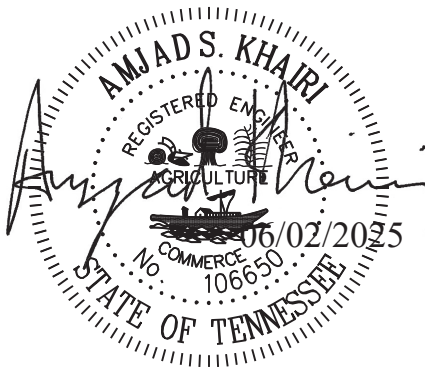
PROJECT NO.		YEAR		SHEET NO.	
43005-4249-04		2025		4B	
REVISIONS					
NO.	DATE	BY	BRIEF DESCRIPTION		
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EPSC NOTES

SR13 OVER DUCK RIVER
BRIDGE I.D. NO. 43SR0130013
BRIDGE NO. 43-SR013-7.71

HUMPHREYS COUNTY
2025

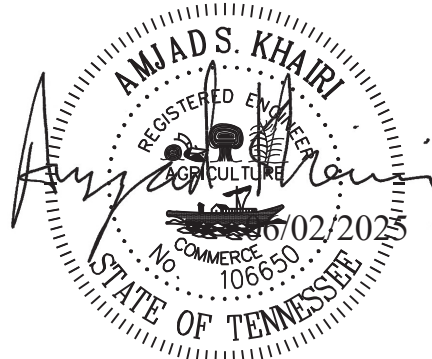


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WORKSPACE: TDOT Bridge (2025)
43205\B01428 - TDOT Humphreys Co SR 13 Duck River Drawings\BRC\08 - Certified (2025 Letting)\01 - Construction - 005.dgn



REVISIONS				TYPE	YEAR	PROJECT NO.	SHEET NO.
NO.	DATE	BY	BRIEF DESCRIPTION	PS&E	2025	43005-4249-04	5

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISION	DESCRIPTION	STA./LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	AN ASBESTOS CONTAINING MATERIAL (ACM) SURVEY WAS COMPLETED ON BRIDGE NO.43SR0130013 SR-13 OVER DUCK RIVER LM 7.71 (43-SR013-07.71). NO ACM WAS DETECTED. PLEASE SEE THE REPORT FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03	N/A
EDEC001	ENVIRONMENTAL DIVISION, ECOLOGY SECTION	IN ACCORDANCE WITH THE MOA BETWEEN USFWS, FHWA, AND TDOT ADDRESSING CLIFF SWALLOW AND BARN SWALLOW NESTING SITES, 09/16/2020, CLIFF SWALLOW AND BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) WILL NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31, FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (E.G., CLOSING OFF AREA USING NETTING).	N/A
EDEC002	ENVIRONMENTAL DIVISION, ECOLOGY SECTION	TDOT COMMITS TO NO IN-STREAM ACTIVITIES FROM MARCH 15TH THROUGH AUGUST 31ST TO AVOID IMPACTS DURING THE MOST SENSITIVE TIME PERIODS FOR THE FEDERAL AND STATE LISTED SPECIES IDENTIFIED WITHIN OR NEAR THE PROPOSED PROJECT FOOTPRINT PER USFWS AND TWRA.	N/A
EDEC003	ENVIRONMENTAL DIVISION, ECOLOGY SECTION	TDOT ENVIRONMENTAL SHALL BE NOTIFIED AT LEAST TWO WEEKS PRIOR TO BEGINNING ANY IN-STREAM WORK, SO THAT TDOT ECOLOGISTS MAY PERFORM MUSSEL SWEEPS IN THE FOOTPRINT OF THE PROPOSED PROJECT. THIS NOTIFICATION SHALL BE SENT TO TDOT ENVIRONMENTAL VIA E-MAIL AT TDOT.ENV.ECOLOGY@TN.GOV. THE CONTRACTOR SHOULD BE READY TO BEGIN CONSTRUCTION OF IN-STREAM STRUCTURES IMMEDIATELY AFTER THE SWEEPS ARE COMPLETED. THIS ACTIVITY MUST BE CONDUCTED OUTSIDE OF THE IN-STREAM PROHIBITION TIMEFRAME GIVEN BY USFWS AND TWRA TO AVOID IMPACTS TO THE SPECIES OF CONCERN IDENTIFIED FOR THE PROPOSED PROJECT.	N/A
EDEC004	ENVIRONMENTAL DIVISION, ECOLOGY SECTION	NO EQUIPMENT OR HAUL ROADS SHALL BE PLACED WITHIN STR-1 (DUCK RIVER) FOR CONSTRUCTION OF PROPOSED REPAIRS TO PIER 1 AND PIER 2, EXCEPT FOR BARGES. ALL WORK SHALL BE PERFORMED FROM BRIDGE DECK OR BARGES WITHIN THE RIVER AS DESCRIBED IN THE SCOPE OF WORK FOR THE PROPOSED PROJECT.	N/A
EDEC005	ENVIRONMENTAL DIVISION, ECOLOGY SECTION	EQUIPMENT STAGING AREA SHOULD BE IN AN UPLAND LOCATION A MINIMUM OF 150 FEET (EXCEPT FOR CRANE EQUIPMENT) FROM STR-1 (DUCK RIVER). ANY EQUIPMENT PARKED OVERNIGHT OR FOR ANY PERIOD OF INACTIVITY, SUCH AS NIGHTS, WEEKENDS, AND OTHER NON-WORKING HOURS, MUST BE MOVED TO THE EQUIPMENT STAGING AREA. DOCUMENTATION OF DAILY EQUIPMENT INSPECTIONS SHOULD BE RETAINED IN/ON EQUIPMENT AND MADE AVAILABLE TO TDOT ENVIRONMENTAL UPON REQUEST.	N/A
EDEC006	ENVIRONMENTAL DIVISION, ECOLOGY SECTION	ALL EQUIPMENT OPERATED WITHIN 150 FEET OF THE DUCK RIVER SHOULD HAVE ATTACHED ABSORBENT PADS DURING BOTH OPERATIONAL AND NON-OPERATIONAL ACTIVITIES TO PREVENT FUELS OR OTHER PETROLEUM PRODUCTS FROM ENTERING STR-1 (DUCK RIVER).	N/A
EDEC007	ENVIRONMENTAL DIVISION, ECOLOGY SECTION	ANY EQUIPMENT OR PROJECT COMPONENT FAILURES THAT MAY RESULT IN UNAUTHORIZED MATERIALS ENTERING THE STR-1 (DUCK RIVER) SHOULD BE REPORTED TO THE TDOT ENVIRONMENTAL DIVISION BY CONTACTING TRENT THOMAS AT 615-483-1538 IMMEDIATELY. ANY FAILURES OF THIS NATURE MUST BE REMEDIED WITHIN 24 HOURS.	N/A
EDEC008	ENVIRONMENTAL DIVISION, ECOLOGY SECTION	ALL ROCK MATERIALS TRANSPORTED TO THE SITE AND INTENDED FOR PLACEMENT AROUND PIER 1 AND PIER 2 FOR ARMORING SHALL BE FREE OF EXCESSIVE FINES.	N/A
EDEC009	ENVIRONMENTAL DIVISION, ECOLOGY SECTION	DURING ALL ACTIVITIES INVOLVING THE POURING OF CONCRETE AROUND PIER 1, A TDOT DISTRICT OPERATIONS SUPERVISOR OR APPOINTEE WITH AUTHORITY TO HALT CONSTRUCTION WILL BE PRESENT TO VISUALLY DETERMINE IF THERE IS A PROBLEM. IF SPILLAGE OR LEAKAGE OF CONCRETE INTO THE STREAM IS OBSERVED, THE LEAK WILL BE FIXED IMMEDIATELY AND POURING WILL CEASE AS SOON AS A SAFE STOPPING POINT IS REACHED. TDOT ENVIRONMENTAL SHALL BE NOTIFIED IMMEDIATELY UPON ANY INDICATION OF A SPILL BY CONTACTING TRENT THOMAS AT 615-483-1538.	N/A
EDEC011	ENVIRONMENTAL DIVISION, ECOLOGY SECTION	A COPY OF THE SWPPP WILL BE RETAINED AT THE CONSTRUCTION SITE FROM PROJECT INCEPTION UNTIL FINAL STABILIZATION. THE SWPPP SHOULD BE STRICTLY FOLLOWED AND UPDATED AS NECESSARY. IN THE EVENT OF SPILL, TDOT ENVIRONMENTAL SHALL BE NOTIFIED IMMEDIATELY BY CONTACTING TRENT THOMAS AT 615-483-1538.	N/A



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING AND DEVELOPMENT

PROJECT
COMMITMENTS



LIST OF DRAWINGS

LIST OF REFERENCE DRAWINGS

 ALL REFERENCE DRAWINGS TO
BE PRINTED WITH THE PLANS

[illegible]

EXISTING HAUL ROAD

EXISTING CONSTRUCTION
ENTRANCE/EXIT




ELEVATION
SCALE: 1" = 75'-0"

ANY WORK WITHIN THE WETLAND/STREAM CHANNEL AREA (E.G., FOR PIER FOOTING, RIPRAP PLACEMENT, MULTI-BARREL CULVERT BRIDGE CONSTRUCTION, ETC.) SHALL BE SEPARATED FROM FLOWING WATER OR EXPECTED FLOW PATH AND PERFORMED DURING LOW FLOW CONDITIONS. ALL ITEMS USED WITHIN THE WETLAND/STREAM CHANNEL AREA FOR DIVERSION OF FLOW (OR EXPECTED FLOW), UNLESS SPECIFIED IN THE PLANS, SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE COST OF THE OTHER ITEMS. THE NOTE EXCLUDES ANY ITEMS SPECIFIED IN THE PLANS FOR USE WITH EC-STR-31 (ECM-STR-31) AND EC-STR-32 (ECM-STR-32).

THE CONTRACTOR SHALL USE ANY MEASURES NECESSARY TO ENSURE THAT NO CONSTRUCTION ACTIVITY WILL OCCUR IN, NOR THAT ANY CONSTRUCTION EQUIPMENT OR MATERIALS WILL ENTER, ANY PORTION OF (WTL-1 AND WTL-2) AND THAT THE STREAM/WETLANDS AND SURROUNDING VEGETATION WILL NOT BE DISTURBED AND ARE PROTECTED FROM SEDIMENT AND OTHER POLLUTANTS EXCEPT AT THE PERMITTED LOCATIONS.

REPAIR UNDERMINED AREA AT PIER NO.1 BY PROVIDING CONTRACTOR'S DESIGNED FORMING AND POURING USING TREMIE CONCRETE.FOR NOTES AND DEATILS SEE DWG.NO.BR-131-147.

POUR CONCRETE TO FILL THE VOIDS AROUND AND UNDERNEATH THE TREMIE SEAL
FOUNDATION AT PIER NO. 1.

 PROVIDE NEW CLASS C RIPRAP AT PIER NO.1 AND PIER NO.2 TO PROVIDE SCOUR PROTECTION. THE CONTRACTOR HAS THE OPTION TO UTILIZE THE CLASS C RIPRAP TO SUPPORT THE FORMS AT PIER NO.1 DURING CONSTRUCTION. FOR NOTES AND DETAILS, SEE DWG. NO. BR-131-147.

CLEAN DEBRIS AT PIERS NO.1 AND 2.COST TO BE INCLUDED UNER ITEM NO. 604-10.15. STRUCTURAL REPAIRS (FOUNDATIONS).

PROVIDE NEW THIN EPOXY OVERLAY FOR THE ENTIRE BRIDGE LENGTH AND WIDTH.
FOR NOTES AND DETAILS, SEE DWG. NO. BR-131-141.

PROVIDE PARTIAL DEPTH DECK REPAIRS AT FIELD DESIGNATED LOCATIONS. FOR
NOTES AND DETAILS, SEE DWG. NO. BR-131-147.

LAYOUT OF BRIDGE TO
BE REPAIRED

SR13 OVER DUCK RIVER
BRIDGE I.D. NO. 43SR0130013
BRIDGE NO. 43-SR013-7.71

HUMPHREYS COUNTY
2025



DESIGNED BY D. P. ERICKSON DATE DEC. 2020
 DRAWN BY C. W. THOMAS DATE DEC. 2020
 SUPERVISED BY J. H. RUDELL DATE DEC. 2020
 CHECKED BY A. J. KHAIRI DATE DEC. 2020

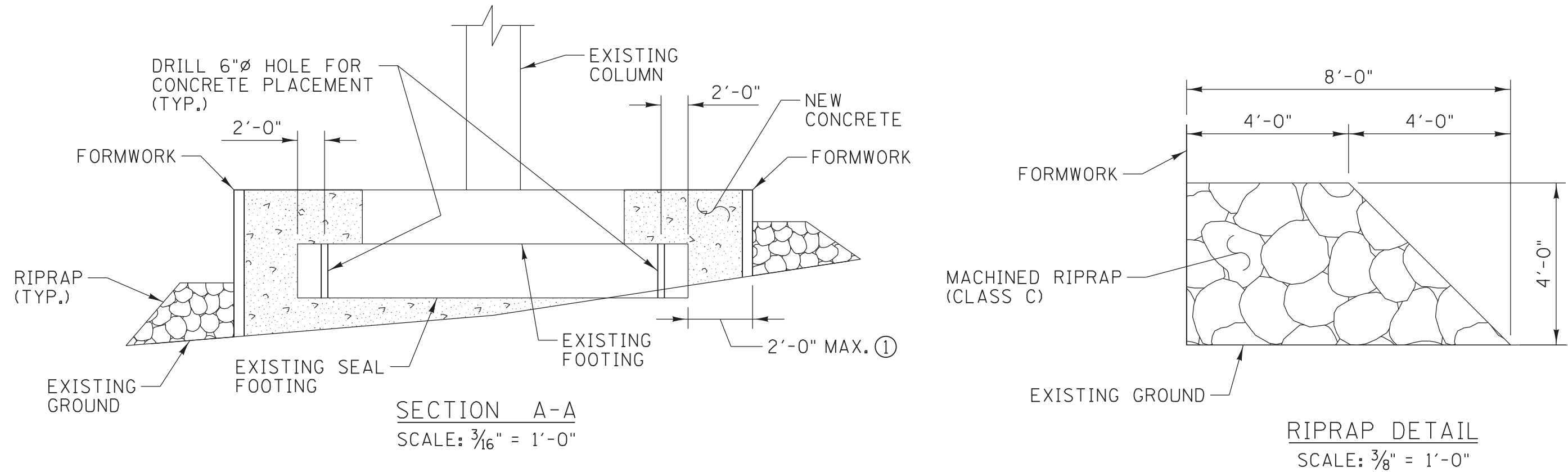
TN D.O.T. ENGINEERING SUPERVISOR J. BENDER

6/27/2025 11:44:48 AM
WORKSPACE: TDOT Bridge (2020)
C:\2025\B01428 - TDOT Humphreys Co. SR 13 Duck River Drawings\BRO\08 - Certified (2025 Letting)\02 - Structures - BR-131-146.dgn



DESIGNED BY D. P. ERICKSON DATE DEC. 2020
DRAWN BY C. W. THOMAS DATE DEC. 2020
SUPERVISED BY J. H. RUDELL DATE DEC. 2020
CHECKED BY A. J. KHAIRI DATE DEC. 2020

TN D.O.T. ENGINEERING SUPERVISOR J. BENDER



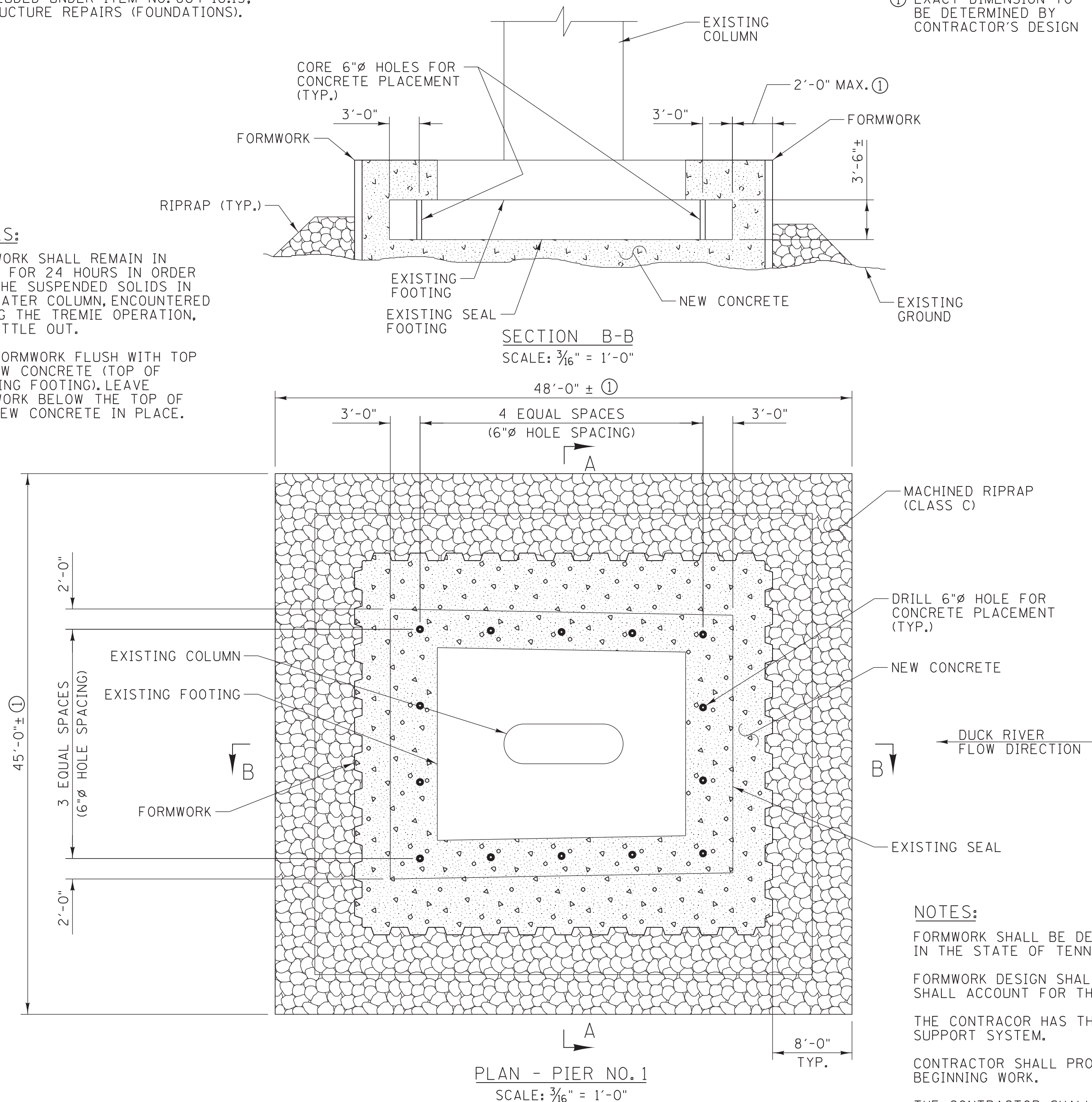
NOTE:

ALL COST, MATERIALS AND LABOR REQUIRED TO PERFORM WORK SHALL BE INCLUDED UNDER ITEM NO. 604-10.15, STRUCTURE REPAIRS (FOUNDATIONS).

NOTES:

FORMWORK SHALL REMAIN IN PLACE FOR 24 HOURS IN ORDER FOR THE SUSPENDED SOLIDS IN THE WATER COLUMN, ENCOUNTERED DURING THE TREMIE OPERATION, TO SETTLE OUT.

CUT FORMWORK FLUSH WITH TOP OF NEW CONCRETE (TOP OF EXISTING FOOTING). LEAVE FORMWORK BELOW THE TOP OF THE NEW CONCRETE IN PLACE.



① EXACT DIMENSION TO BE DETERMINED BY CONTRACTOR'S DESIGN

NOTES:

FORMWORK SHALL BE DESIGNED AND STAMPED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF TENNESSEE.

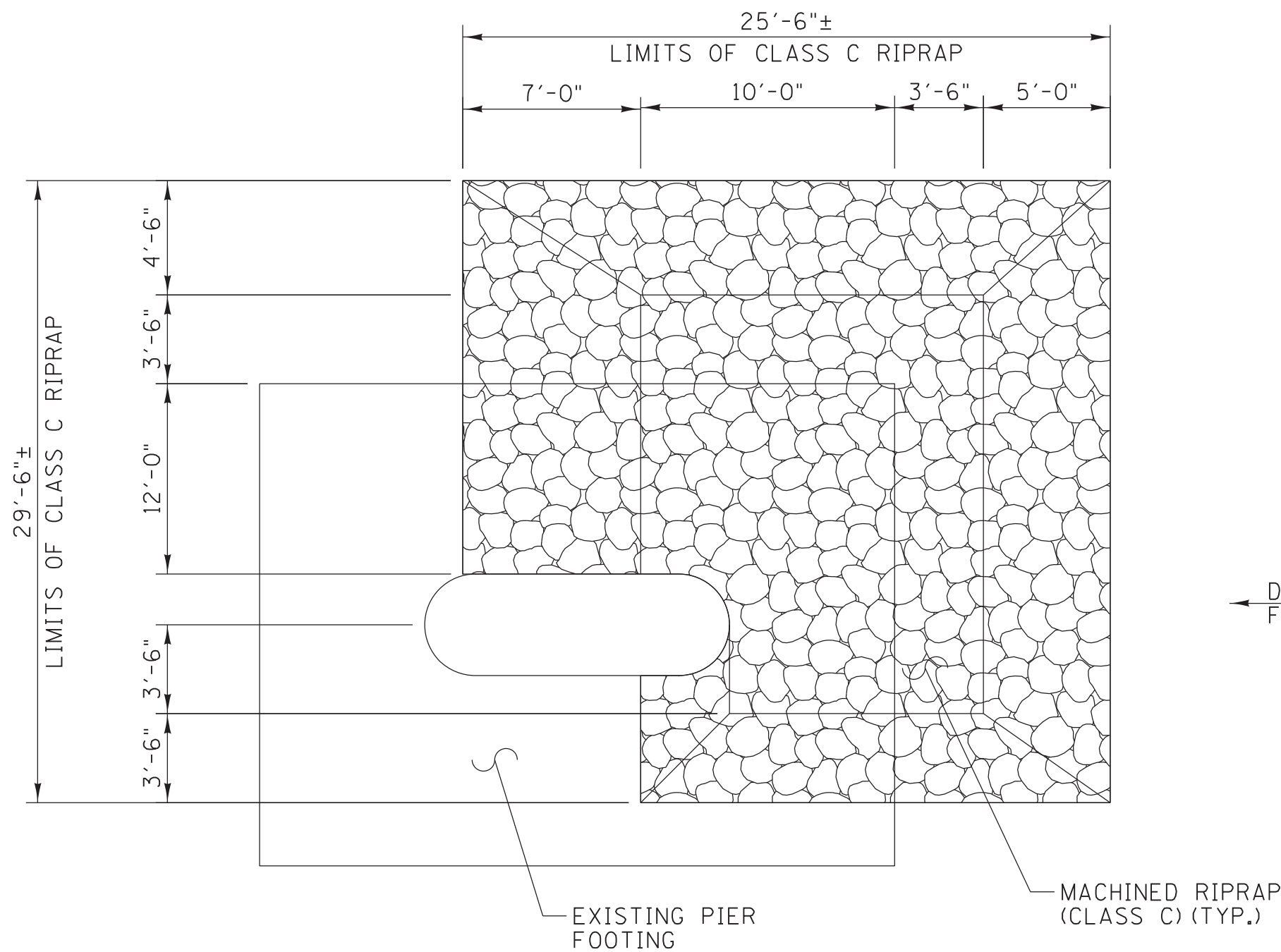
FORMWORK DESIGN SHALL ACCOUNT FOR ALL DEAD, LIVE, STREAM AND WIND LOADS AND SHALL ACCOUNT FOR THE WEIGHT OF THE CONCRETE PRIOR TO HARDENING.

THE CONTRACTOR HAS THE OPTION TO UTILIZE THE RIPRAP AS PART OF THE FORMWORK SUPPORT SYSTEM.

CONTRACTOR SHALL PROVIDE TDOT WITH PROPOSED METHOD OF CONSTRUCTION PRIOR TO BEGINNING WORK.

THE CONTRACTOR SHALL TAKE FULL RESPONSIBILITY FOR THE DESIGN AND CONSTRUCTION OF THE PROPOSED METHOD OF REPAIR.

COST OF CORING 14 - 6"Ø HOLES TO BE INCLUDED UNDER ITEMS BID ON.

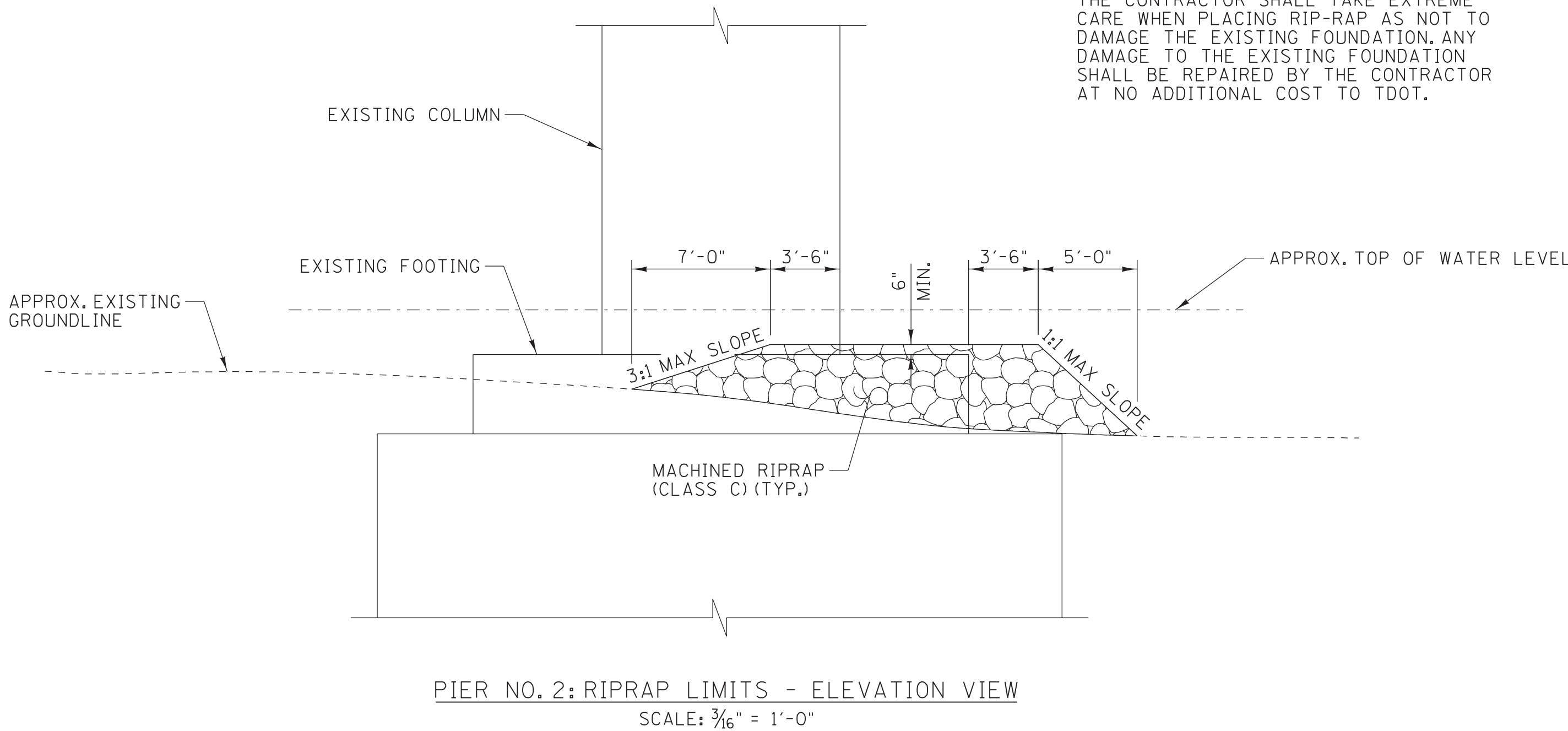


PIER NO. 2: RIPRAP LIMITS - PLAN VIEW
SCALE: 3/16" = 1'-0"

NOTES:

ALL COSTS OF LABOR, MATERIALS, EQUIPMENT AND INCIDENTALS TO HAUL AND PLACE CLASS C RIP-RAP AT PIER NO. 2 SHALL BE PAID FOR UNDER ITEM NO. 709-05.09, MACHINED RIP-RAP (CLASS C), TON.

THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN PLACING RIP-RAP AS NOT TO DAMAGE THE EXISTING FOUNDATION. ANY DAMAGE TO THE EXISTING FOUNDATION SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO TDOT.



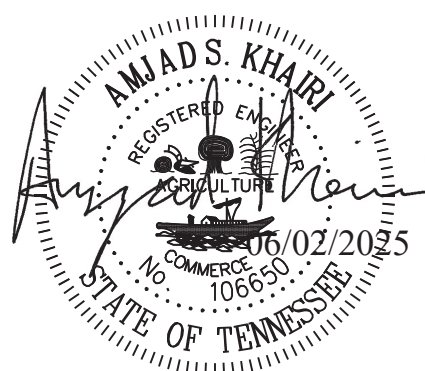
PIER NO. 2: RIPRAP LIMITS - ELEVATION VIEW
SCALE: 3/16" = 1'-0"

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS

SR13 OVER DUCK RIVER
BRIDGE I.D. NO. 43SR0130013
BRIDGE NO. 43-SR013-7.71

HUMPHREYS COUNTY
2025

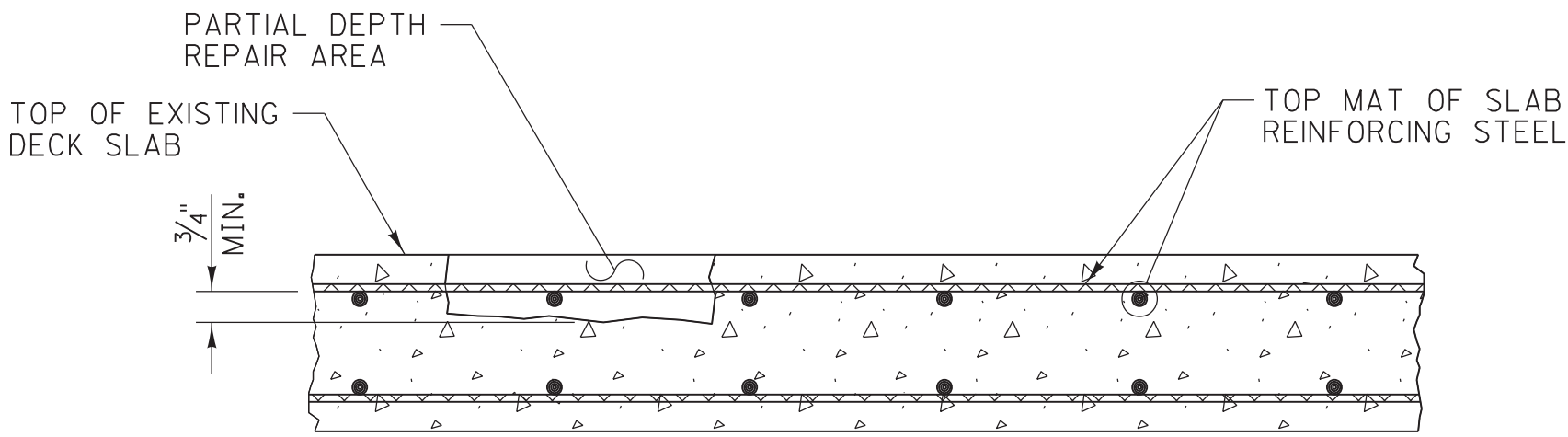


6/27/2025 3:38 PM
WORKSPACE: T001 Bridge (2025)
C:\2025\B01428 - T001 Humphreys Co. SR 13 Duck River Drawings\BRO\08 - Certified (2025 Letting)\02 - Structures - BR-131-147.dgn



DESIGNED BY J. L. HALBROOK DATE AUG. 2024
DRAWN BY J. L. HALBROOK DATE AUG. 2024
SUPERVISED BY J. H. RUDELL DATE AUG. 2024
CHECKED BY A. J. KHAIRI DATE AUG. 2024

TN D.O.T. ENGINEERING SUPERVISOR J. BENDER



DETAIL SHOWING PARTIAL DEPTH REPAIR

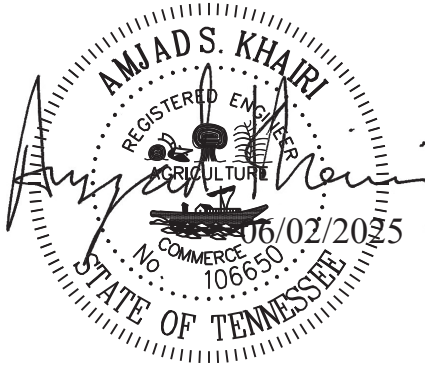
NOTE:
ITEM NO. 604-10.50 SHALL BE BID WITH THE CONTINGENCY THAT THIS ITEM MAY BE INCREASED OR DECREASED AS DIRECTED BY THE ENGINEER.

PROJECT NO.	YEAR	SHEET NO.	
43005-4249-04	2025		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

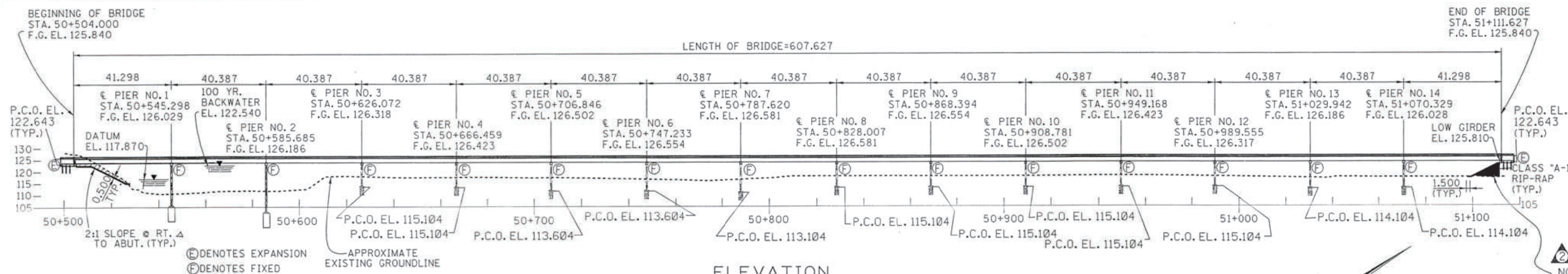
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS

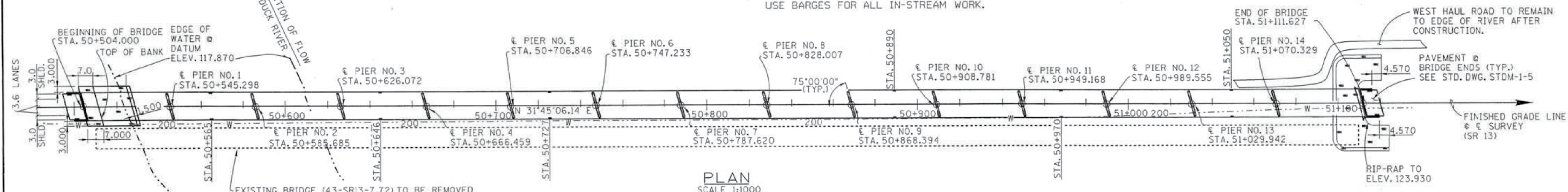
SR13 OVER DUCK RIVER
BRIDGE I.D. NO. 43SR0130013
BRIDGE NO. 43-SR013-7.71



HUMPHREYS COUNTY
2025



CROSS-HATCHED AREA TO BE EXCAVATED WITHIN THE LIMITS OF R.O.W. AND PAID FOR UNDER ROADWAY ITEM NO. 203.01

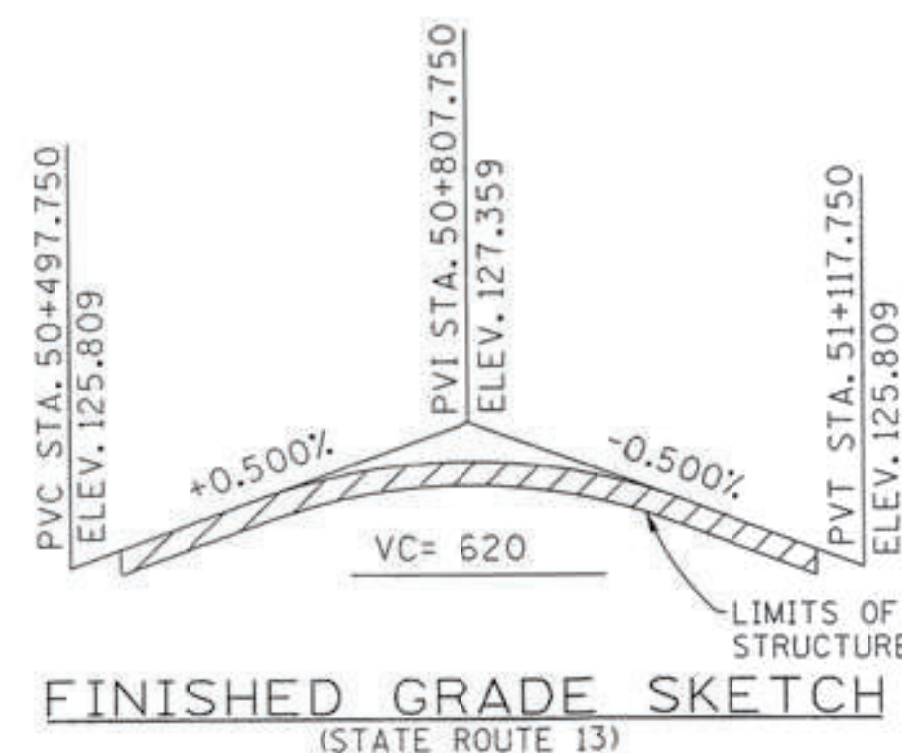


PLAN
SCALE 1:1000

- DENOTES: END OF BRIDGE DRAINS.
SEE STD. DWG. STD-M-1-6, 7 AND 9.
- DENOTES: STANDARD DECK DRAINS (GRATE INLET TYPE I)
SEE STD. DWG. STD-M-1-2.

HYDRAULIC DATA

DRAINAGE AREA = 6634.230 km²
DESIGN DISCHARGE (100 YR.) = 5968.600 m³/s
WATER AREA PROVIDED BELOW EL. 121.610 = 2823.000 m²
100 YEAR VELOCITY = 2.11 m/s
100 YEAR BRIDGE BACKWATER = 0.120 m @ EL. 122.540
ROADWAY OVERTOPPING EL. = 125.000
500 YEAR DISCHARGE = 7661.000 m³/s @ EL. 123.230



LIST OF STANDARD DRAWINGS	DWG. NO.	LAST REV. DATE
BRIDGE RAILING CONCRETE PARAPET	STD-M-1-1	7-31-00
SLIDER PLATES AND DECK DRAINS	STD-M-1-2	1-5-01*
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STD-M-1-5	7-31-00
BRIDGE END DRAIN DETAILS WITH PAVEMENT	STD-M-1-6	4-28-97
AT BRIDGE ENDS	STD-M-1-7	7-31-00
BRIDGE END DRAIN DETAILS WITH PAVEMENT	STD-M-1-9	6-10-96
AT BRIDGE ENDS	STD-M-1-9	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS	STD-M-4-1	5-21-99
GENERAL DETAILS	STD-M-4-2	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS	STD-M-4-3	6-10-96
GENERAL DETAILS	STD-M-4-4	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS	STD-M-5-1	6-10-96
CONSTRUCTION DETAILS	STD-M-5-2	5-21-99
STANDARD PILE DETAILS	STD-M-6-1	5-21-99
STANDARD PILE DETAILS	STD-M-9-1	6-10-96
STANDARD SEISMIC DETAILS	STD-M-10-1	6-10-96
REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STD-M-14-1	7-31-00
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	RD-M-SA-1	5-27-98
STD. DETAILS AND INT. DIAPH.		
DETAILS FOR BULB-TEE BEAMS		
SLOPE PROTECTION		

LIST OF SPECIAL PROVISIONS	DWG. NO.	LAST REV. DATE
APPROVAL OF SHOP DRAWINGS	105A	12-15-97
MODULAR ROADWAY EXPANSION DEVICE	604M	3-22-99
FOUNDATION PREPARATION	604F	8-08-01

LIST OF DRAWINGS	DWG. NO.	LAST REV. DATE
LAYOUT OF BRIDGE	M-373-43	6-08-04
GENERAL NOTES	M-373-44	6-08-04
ESTIMATED QUANTITIES	M-373-45	6-08-04
FOUNDATION DATA	M-373-46	6-08-04
FOUNDATION DATA	M-373-47	6-08-04
SUPERSTRUCTURE	M-373-48	6-08-04
SUPERSTRUCTURE DETAILS	M-373-49	6-08-04
SUPERSTRUCTURE DETAILS	M-373-50	6-08-04
SUPERSTRUCTURE DETAILS	M-373-51	6-08-04
BEARING DETAILS	M-373-52	9-09-02
PRESTRESSED BEAM DETAILS	M-373-53	9-09-02
ABUTMENT NOS. 1 & 2	M-373-54	9-09-02
ABUTMENT NOS. 1 & 2 DETAILS	M-373-55	9-09-02
ABUTMENT NOS. 1 & 2 DETAILS	M-373-56	9-09-02
ABUTMENT NOS. 1 & 2 DETAILS	M-373-57	9-09-02
PIER NOS. 1 & 2	M-373-58	9-09-02
PIER NOS. 1 & 2 DETAILS	M-373-59	9-09-02
PIER NOS. 3 THRU 14	M-373-60	9-09-02
PIER NOS. 3 THRU 14 DETAILS	M-373-61	9-09-02
FINAL FOUNDATION	M-373-62	9-09-02
FINAL FOUNDATION	M-373-63	9-09-02
BILL OF STEEL	M-373-64	9-09-02
BILL OF STEEL	M-373-65	9-09-02
BILL OF STEEL	M-373-66	9-09-02
BILL OF STEEL	M-373-67	9-09-02
BILL OF STEEL	M-373-68	9-09-02

■ DENOTES: THIS DRAWING IS TO BE PRINTED WITH PLANS.



NOTE: ALL DIMENSIONS SHOWN IN METERS UNLESS OTHERWISE NOTED.

REFERENCE DRAWINGS
E-5-35; E-2-4; E-2-56; E-2-150, 151 & 152.
E-5-36 & 37; C-10-54, 55 & 58.

2019 ADT = 3995
13,200 ROADWAY WITH STD-M-1-1 BRIDGERAIL
DESIGN SPEED = 100 km/h

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
LAYOUT OF BRIDGE
STATE ROUTE 13

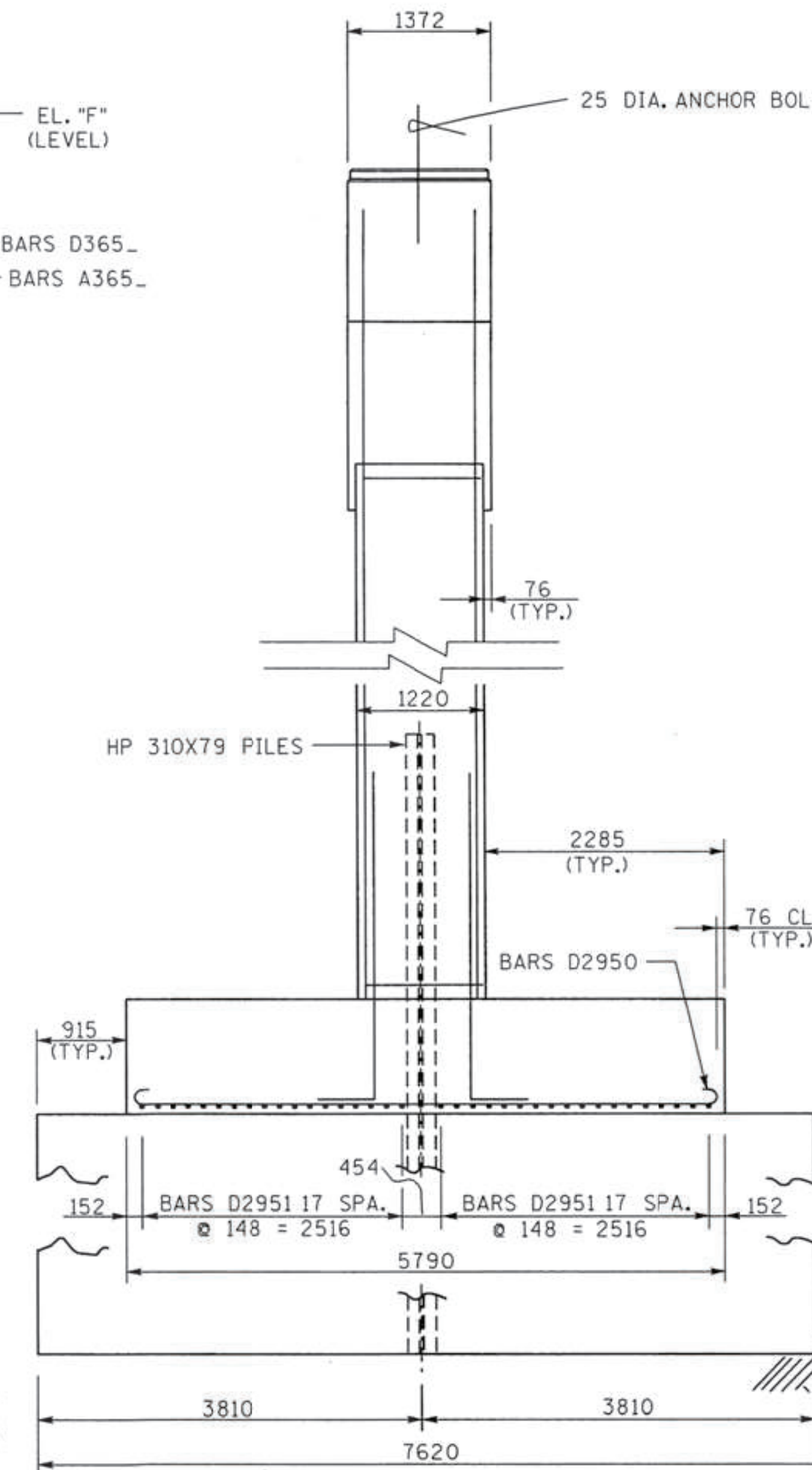
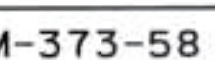
OVER
DUCK RIVER
BRIDGE I.D. NO. 43SR137.72
STATION 50+807.750
LOG MILE 7.72
HUMPHREYS COUNTY
2001



CORRECT *Edward P. Wasserman*
ENGINEER OF STRUCTURES

M-373-43

MACHINED RIP-RAP (CLASS A-1) = 734/m³



SWPPP INDEX OF SHEETS

DESCRIPTION	SHT.
1. SWPPP REQUIREMENTS (5.0.)	1
2. SITE DESCRIPTION (5.5.1.)	1
3. ORDER OF CONSTRUCTION ACTIVITIES (5.5.1.a)	1
4. STREAM, OUTFALL, WETLAND, TMDL AND ECOLOGY INFORMATION	1-2
5. EROSION PREVENTION AND SEDIMENT CONTROL (EPSC) MEASURES (5.5.3.)	2-3
6. FLOCCULANTS (3.5.3.1.b)	3
7. UTILITY RELOCATION	3-4
8. MAINTENANCE AND INSPECTION	4
9. SITE ASSESSMENTS (5.5.3.8.)	4
10. STORMWATER MANAGEMENT (5.5.3.11.h)	4-5
11. NON-STORMWATER DISCHARGES (5.5.3.12.)	5
12. SPILL PREVENTION, MANAGEMENT AND NOTIFICATION (5.5.3.7.c, 6.1)	5
13. RECORD-KEEPING	6
14. SITE WIDE/PRIMARY PERMITTEE CERTIFICATION (8.7.5.)	7
15. SECONDARY PERMITTEE (OPERATOR) CERTIFICATION (8.7.6.)	7
16. ENVIRONMENTAL PERMITS (1.5.2.)	7
17. OUTFALL TABLE (5.5.1.c, 6.4.1.e, 6.4.1.f)	8

NOTE: CITATIONS IN PARENTHESIS INDICATE SECTIONS OF THE CURRENT CGP.

1. SWPPP REQUIREMENTS (5.0.)

1.1. HAS THE SWPPP TEMPLATE BEEN PREPARED BY AN INDIVIDUAL THAT HAS THE FOLLOWING LICENSING AND/OR CERTIFICATIONS (5.2)?

☒ YES (CHECK ALL THAT APPLY BELOW) OR ☐ NO

☐ CERTIFIED PROFESSIONAL IN EROSION AND SEDIMENT CONTROL (CPESC)
☐ A TN LICENSED PROFESSIONAL ENGINEER OR LANDSCAPE ARCHITECT
☒ HAS SUCCESSFULLY COMPLETED TDEC LEVEL II COURSE

1.2. DO THE EPSC PLANS INVOLVE STRUCTURAL DESIGN, HYDRAULIC, HYDROLOGIC OR OTHER ENGINEERING CALCULATIONS FOR EPSC STRUCTURAL MEASURES (E.G. SEDIMENT BASINS) (5.2)? YES ☐ NO ☒

IF YES, HAVE THE EPSC PLANS BEEN PREPARED, STAMPED AND CERTIFIED BY A TN LICENSED PROFESSIONAL ENGINEER OR LANDSCAPE ARCHITECT? ☐ YES ☐ NO

1.3. DO THE PROJECT STORMWATER OUTFALLS DISCHARGE INTO THE FOLLOWING (6.4.1.)? ☐ YES (CHECK ALL THAT APPLY BELOW) ☒ NO

☐ WATERS WITH UNAVAILABLE PARAMETERS (303d FOR SILTATION)
☐ EXCEPTIONAL TENNESSEE WATERS (ETW)
2. SITE DESCRIPTION (5.5.1.)

2.1. PROJECT LIMITS (5.5.1.f): REFER TO TITLE SHEET

2.2. TOTAL PROJECT AREA (5.5.1.b): 8.30 ACRES

2.3. TOTAL AREA TO BE DISTURBED (5.5.1.b): 0 ACRES

2.4. PROJECT DESCRIPTION (5.5.1.a):

TITLE: SR-13 BRIDGE REPAIR OVER DUCK RIVER, LM 7.71
COUNTY: HUMPHREYS
PIN: 101560.01

2.5. SITE MAP(S) (3.2.2.): REFER TO TITLE SHEET

2.6. DESCRIPTION OF EXISTING SITE TOPOGRAPHY (5.5.1.c): REFER TO EXISTING CONTOURS SHEET(S) 8, 9, & 10, DRAINAGE MAP SHEET(S) 8, 9, & 10, USGS QUAD MAP, AND THE OUTFALL TABLE IN SECTION 4.2.

2.7. MAJOR SOIL DISTURBING ACTIVITIES (5.5.1.a) (CHECK ALL THAT APPLY):

☒ CLEARING AND GRUBBING
☐ EXCAVATION
☐ CUTTING AND FILLING
☐ FINAL GRADING AND SHAPING
☐ UTILITIES
☐ OTHER (DESCRIBE): _____

2.8. NO MORE THAN 50 ACRES OF ACTIVE SOIL DISTURBANCE IS ALLOWED AT ANY TIME DURING THE CONSTRUCTION OF THE PROJECT.

2.9. ARE THERE ANY SEASONAL LIMITATIONS ON WORK? ☒ YES ☐ NO
IF YES, LIST THE CORRESPONDING PLAN SHEET: 5

- 2.10. WAS ROW FINALIZED PRIOR TO FEBRUARY 1, 2010 (4.1.2.2)?

☐ YES _____ (DATE) ☒ NO

IF ROW WAS FINALIZED PRIOR TO FEBRUARY 1, 2010, THIS PROJECT IS CONSIDERED A PRE-APPROVED SITE (4.1.2.2)
- 2.11. SOIL PROPERTIES (5.5.1.d, 5.5.3.3.d, 5.5.3.6.b).

SOIL PROPERTIES FOR THE PRIMARY SOILS ARE LISTED IN THE TABLE BELOW.

SOIL PROPERTIES			
PRIMARY SOIL NAME	HSG	% OF SITE	ERODIBILITY (k value)
Bcz - Bodine gravelly silt loam, 20 to 40 percent slopes	A	2.7	0.24
HI - Huntington silt loam	B	49.5	0.37
Hlk - Huntington silt loam, dark-subsoil	B	0.3	0.37
Hs - Huntington silty clay loam	C	20.8	0.37
W - Water	-	15.0	-
Wc - Wolftever silty clay loam	C	11.7	0.37

- 2.12. IS ACID PRODUCING ROCK (APR) (i.e. PYRITE) LOCATED WITHIN THE PROJECT LIMITS? ☐ YES ☒ NO

2.12.1. IF YES TO SECTION 2.13, HAVE APR LOCATIONS BEEN IDENTIFIED WITHIN THE CONSTRUCTION PLANS AND/OR THE GEOTECHNICAL REPORT? ☐ YES ☐ NO; AND

2.12.2. IF YES TO SECTION 2.12.1, HAS A SPECIAL HANDLING PLAN AND/OR ADAPTIVE MANAGEMENT PLAN (AMP) BEEN PREPARED FOR THE PROJECT? ☐ YES ☐ NO ☐ N/A (TDOT SP107L WILL BE APPLIED.)
- 2.13. PROJECT RUNOFF COEFFICIENTS AND AREA PERCENTAGES (5.5.3.6.a).

RUNOFF COEFFICIENTS FOR EXISTING CONDITIONS				
AREA TYPE	AREA(AC)	PERCENTAGE OF TOTAL AREA (%)	RUNOFF CN	C FACTOR
IMPERVIOUS	2.26	27.2		0.90
PERVIOUS	6.04	72.8		0.40
WEIGHTED CURVE NUMBER OR C-FACTOR =				0.54

RUNOFF COEFFICIENTS FOR POST-CONSTRUCTION CONDITIONS				
AREA TYPE	AREA(AC)	PERCENTAGE OF TOTAL AREA (%)	RUNOFF CN	C FACTOR
IMPERVIOUS	2.26	27.2		0.90
PERVIOUS	6.04	72.8		0.40
WEIGHTED CURVE NUMBER OR C-FACTOR =				0.54

3. ORDER OF CONSTRUCTION ACTIVITIES (5.5.1.a)

CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO: MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION. NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF THEIR OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE ENGINEER. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE ORDER OF CONSTRUCTION ACTIVITIES AND THE BASIC EPSC DEVICES DEPICTED ON THE EPSC PLAN CONTAINED WITHIN THE APPROVED SWPPP.

3.1. SPECIAL SEQUENCING REQUIREMENTS (SEE SHEETS N/A)

3.2. INSTALL STABILIZED CONSTRUCTION EXITS.

3.3. INSTALL PERIMETER PROTECTION WHERE RUNOFF SHEET FLOWS FROM THE SITE.

3.4. INSTALL INITIAL EPSC MEASURES BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CULVERT OR BRIDGE CONSTRUCTION, CUTTING,
- FILLING, OR ANY OTHER EARTHWORK OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.

3.5. PERFORM CLEARING AND GRUBBING (NOT MORE THAN TWO WEEKS PRIOR TO GRADING OR EARTH-MOVING. REFER TO THE STABILIZATION PRACTICES BELOW.).

3.6. REMOVE AND STORE TOPSOIL.

3.7. STABILIZE DISTURBED AREAS WITHIN 2 WEEKS OF COMPLETING ANY STAGE AND/OR PHASE OF ACTIVITY (STEEP SLOPES SHALL BE STABILIZED WITHIN 1 WEEK AFTER CONSTRUCTION ACTIVITY HAS TEMPORARY OR PERMANENTLY CEASED).

3.8. INSTALL UTILITIES, STORM SEWERS, CULVERTS AND BRIDGE STRUCTURES.

3.9. INSTALL INLET AND CULVERT PROTECTION ONCE STRUCTURES ARE IN PLACE AND CAPABLE OF INTERCEPTING FLOW.

3.10. PERFORM FINAL GRADING AND INSTALL BASE STONE.

3.11. COMPLETE FINAL PAVING AND SEALING OF CONCRETE.

3.12. INSTALL TRAFFIC CONTROL AND PROTECTION DEVICES.

3.13. COMPLETE PERMANENT STABILIZATION (TOPSOIL, SEEDING, MULCH, EROSION CONTROL BLANKET, SOD, ETC.)

3.14. REMOVE TEMPORARY EROSION CONTROLS AND ACCUMULATED SEDIMENT FROM AREAS THAT HAVE ESTABLISHED AT LEAST 70 PERCENT UNIFORM PERMANENT VEGETATIVE COVER.

3.15. RE-STABILIZE AREAS DISTURBED BY REMOVAL ACTIVITIES.
4. STREAM, OUTFALL, WETLAND, TMDL AND ECOLOGY INFORMATION
- 4.1. STREAM INFORMATION (5.5.1.h, 5.5.1.i)

4.1.1. WILL CONSTRUCTION AND/OR EROSION PREVENTION AND SEDIMENT CONTROLS IMPACT ANY STREAMS WITHIN THE PROJECT LIMITS? ☒ YES ☐ NO

IF YES, THE IMPACT(S) HAVE BEEN INCLUDED IN THE TOTAL PROJECT IMPACTS AND HAVE BEEN INCLUDED IN THE WATER QUALITY PERMITS.

4.1.2. HAVE ANY OF THE RECEIVING STATE WATERS LESS THAN OR EQUAL TO 1 FLOW MILE DOWN GRADIENT OF THE PROJECT LIMITS BEEN CLASSIFIED BY TDEC AS FOLLOWS (CHECK ALL THAT APPLY):

☐ 303d WITH UNAVAILABLE PARAMETERS FOR SILTATION
☒ EXCEPTIONAL TENNESSEE WATERS (ETW)

4.1.3. RECEIVING WATERS OF THE STATE (5.5.1.h, 5.5.1.j, 5.5.1.k).
- | RECEIVING WATERS OF THE STATE INFORMATION | | | | | |
|---|-------------------------------|--|-----------------|---|--|
| TDOT STATE WATER LABEL FROM EBR | NAME OF RECEIVING STATE WATER | 303d WITH UNAVAILABLE PARAMETERS FOR SILTATION (YES OR NO) | ETW (YES OR NO) | LOCATED WITHIN PROJECT LIMITS (YES OR NO) | LOCATED WITHIN ≤ 1 FLOW MILE DOWN GRADIENT OF PROJECT LIMITS (YES OR NO) |
| STR-1 | DUCK RIVER | NO | YES | YES | YES |
| | | | | | |
- 4.1.4. RECEIVING WATERS OF THE US (NON STATE WATERS) (4.1.2). LIST ANY FEATURE THAT IS IDENTIFIED AS A WET WEATHER CONVEYANCE (TDEC) AND IDENTIFIED AS WATERS OF THE US BY THE ARMY CORPS OF ENGINEERS.
- | WET WEATHER CONVEYANCES THAT ARE WATERS OF THE US | | |
|---|---|--|
| TDOT STATE WATER LABEL FROM EBR | LOCATED WITHIN PROJECT LIMITS (YES OR NO) | LOCATED WITHIN ≤ 1 FLOW MILE DOWN GRADIENT OF PROJECT LIMITS (YES OR NO) |
| | | |
- STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
- STORMWATER
POLLUTION
PREVENTION
PLAN

4.1.5. ARE THERE ANY WATER QUALITY RIPARIAN BUFFER ZONES REQUIRED FOR WATERS OF THE STATE? (5.5.1.i, 6.4.2.)

☐ YES ☒ NO

BUFFER ZONE REQUIREMENTS ARE NOT REQUIRED FOR PRE-APPROVED SITES (4.1.2.2.)

IF YES, THEY HAVE BEEN INCLUDED ON PLAN SHEET(S) ____.

IF YES, CHECK THE APPROPRIATE BOX BELOW FOR SIZE OF BUFFER.

☐ 60-FEET FOR WATERS WITH UNAVAILABLE PARAMETERS AND EXCEPTIONAL TENNESSEE WATERS (ETW) (AVERAGE WIDTH PER SIDE WITH A MINIMUM OF 30-FEET).

A 60 FOOT NATURAL WATER QUALITY RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STATE STREAM WITH THIS DESIGNATION SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 60 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 30 FEET AT ANY MEASURED LOCATION. IF THE CONSTRUCTION SITE ENCOMPASSES BOTH SIDES OF A STREAM, BUFFER AVERAGING CAN BE APPLIED TO BOTH SIDES, BUT MUST BE APPLIED INDEPENDENTLY.

☐ 30-FEET FOR ALL OTHER STREAMS (AVERAGE WIDTH PER SIDE WITH A MINIMUM OF 15-FEET).

A 30 FOOT NATURAL WATER QUALITY RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STATE STREAM SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 30 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 15 FEET AT ANY MEASURED LOCATION. IF THE CONSTRUCTION SITE ENCOMPASSES BOTH SIDES OF A STREAM, BUFFER AVERAGING CAN BE APPLIED TO BOTH SIDES, BUT MUST BE APPLIED INDEPENDENTLY.

☐ 15-FEET FOR ANY WET WEATHER CONVEYANCES IDENTIFIED AS WATERS OF THE US BY THE US ARMY CORPS OF ENGINEERS.

4.1.6. ARE THERE ANY WATER QUALITY RIPARIAN BUFFER ZONES NOT REQUIRED FOR STATE WATERS DUE TO A TDEC ARAP? (1.5.2.)

☒ YES ☐ NO

4.1.7. ARE THERE WATER QUALITY RIPARIAN BUFFER ZONE EXEMPTIONS? (4.1.2.1.) ☐ YES ☒ NO

IF YES, EXISTING CONDITIONS DESCRIPTION: ____

4.1.8. EVERY ATTEMPT SHOULD BE MADE FOR CONSTRUCTION ACTIVITIES TO NOT TAKE PLACE WITHIN THE WATER QUALITY RIPARIAN BUFFER ZONE AND FOR EXISTING FORESTED AREAS TO BE PRESERVED. (4.1.2., 6.4.2.)

4.1.9. BECAUSE OF HEAVY SEDIMENT LOAD ASSOCIATED WITH CONSTRUCTION SITE RUNOFF, WATER QUALITY RIPARIAN BUFFER ZONES ARE NOT SEDIMENT CONTROL MEASURES AND SHOULD NOT BE RELIED UPON AS PRIMARY SEDIMENT CONTROL MEASURES. THE WATER QUALITY RIPARIAN BUFFER ZONE SHALL BE ESTABLISHED BETWEEN THE TOP OF THE STREAM BANK AND THE DISTURBED CONSTRUCTION AREA.

4.1.10. WHERE IT IS NOT PRACTICABLE TO MAINTAIN A FULL WATER QUALITY RIPARIAN BUFFER, BEST MANAGEMENT PRACTICES (BMPs) PROVIDING EQUIVALENT PROTECTION AS THE NATURAL RIPARIAN ZONE MUST BE USED. A JUSTIFICATION FOR USE AND DESIGN EQUIVALENCY SHALL BE DOCUMENTED WITHIN THE SWPPP. THE ENVIRONMENTAL AND ROADWAY DESIGN DIVISIONS SHALL REVIEW AND APPROVE THIS REVISION OF THE SWPPP BEFORE DISTURBANCE OF THE SITE PROCEEDS, UNLESS PREVIOUSLY EXEMPT IN THE NPDES CGP. WHERE ISSUED, ARAP/401 REQUIREMENTS WILL PREVAIL IF IN CONFLICT WITH THESE BUFFER ZONE REQUIREMENTS.

4.2. OUTFALL INFORMATION

4.2.1. OUTFALL TABLE (5.5.1.c). SEE SWPPP SHEET S-8 FOR OUTFALL INFORMATION.

4.2.2. HAVE ALL OUTFALLS BEEN LABELED ON THE EPSC PLAN SHEETS (5.5.1.f)? ☒ YES ☐ NO

4.2.3. HAVE ALL OUTFALLS BEEN LABELED ON A USGS TOPOGRAPHIC MAP INCLUDED IN THE “DOCUMENTATION AND PERMITS” BINDER (3.2.2.)? ☒ YES ☐ NO

4.2.4. WHERE POSSIBLE, HAS NON-PROJECT RUN-ON BEEN DIVERTED AROUND OR THROUGH THE PROJECT TO ELIMINATE CONTACT WITH DISTURBED AREAS OF THE PROJECT AND SEPARATE IT FROM PROJECT RUN-OFF THEREBY REDUCING THE DRAINAGE AREA OF TO THE OUTFALLS IN THIS AREA?

☐ YES ☐ NO ☒ N/A

4.2.5. ARE EQUIVALENT MEASURES BEING SUBSTITUTED FOR A SEDIMENT BASIN(S) OR SEDIMENT TRAP(S)? (5.5.3.5.)

☐ YES ☐ NO ☒ N/A

4.2.6. A SEDIMENT BASIN, OR EQUIVALENT MEASURE(S) WILL BE PROVIDED FOR ANY OUTFALL IN A DRAINAGE AREA:

OF TEN ACRES OR MORE FOR AN OUTFALL(S) THAT DOES NOT DISCHARGE TO A STATE STREAM WITH UNAVAILABLE PARAMETERS OR EXCEPTIONAL TENNESSEE WATERS (ETW). A TEMPORARY (OR PERMANENT) SEDIMENT BASIN OR EQUIVALENT CONTROL MEASURES THAT PROVIDES STORAGE FOR A CALCULATED VOLUME OF RUNOFF FROM A MINIMUM 2-YEAR/ 24-HOUR STORM EVENT, SHALL BE PROVIDED UNTIL PERMANENT STABILIZATION OF THE SITE. (5.5.3.5)

OR

OF FIVE ACRES OR MORE FOR AN OUTFALL(S) THAT DISCHARGES TO A STATE STREAM WITH UNAVAILABLE PARAMETERS OR EXCEPTIONAL TENNESSEE WATERS (ETW). A TEMPORARY (OR PERMANENT) SEDIMENT BASIN THAT PROVIDES STORAGE FOR A CALCULATED VOLUME OF RUNOFF FROM A 5-YEAR/ 24-HOUR STORM EVENT AND RUNOFF FROM EACH ACRE DRAINED, OR EQUIVALENT CONTROL MEASURES, SHALL BE PROVIDED UNTIL PERMANENT STABILIZATION OF THE SITE. (6.4.1.e).

ALL CALCULATIONS RELATED TO DRAINAGE AREAS, RUNOFF COEFFICIENTS, BASIN VOLUMES AND EQUIVALENT CONTROL MEASURES MUST BE PROVIDED IN THE SWPPP (5.5.3.5.)

4.2.7. A SEDIMENT TRAP, OR EQUIVALENT MEASURE(S) WILL BE PROVIDED FOR ANY OUTFALL IN A DRAINAGE AREA:

OF 3.5 - 4.9 ACRES FOR AN OUTFALL(S) THAT DISCHARGES TO A STATE STREAM WITH UNAVAILABLE PARAMETERS (303d SILTATION) OR EXCEPTIONAL TENNESSEE WATERS (ETW). A SEDIMENT TRAP THAT PROVIDES STORAGE FOR A CALCULATED VOLUME OF RUNOFF FROM A 5-YEAR/24-HOUR STORM EVENT AND RUNOFF FROM EACH ACRE DRAINED, OR EQUIVALENT CONTROL MEASURES, SHALL BE PROVIDED UNTIL PERMANENT STABILIZATION OF THE SITE. (6.4.1.f).

IN BOTH INSTANCES, THE ENVIRONMENTAL AND ROADWAY DESIGN DIVISIONS MAY BE CONTACTED TO REVIEW AND CONCUR WITH ANY REVISION OF THE SWPPP BEFORE DISTURBANCE OF THE OUTFALL PROCEEDS.

4.2.8. SEDIMENT STRUCTURES TREATING DRAINAGE AREAS IN EXCESS OF 25 ACRES REQUIRE A SITE-SPECIFIC DESIGN THAT ACCURATELY DEFINES THE SITE HYDROLOGY, SITE-SPECIFIC SEDIMENT LOADING, HYDRAULICS OF THE SITE, AND ADHERES TO ALL TENNESSEE EROSION AND SEDIMENT CONTROL HANDBOOK DESIGN RECOMMENDATIONS FOR SEDIMENT BASINS. (5.5.3.5.)

4.3. WETLAND INFORMATION

WILL CONSTRUCTION AND/OR EROSION AND SEDIMENT CONTROLS IMPACT ANY WETLANDS? ☐ YES ☒ NO

IF YES, THE STRUCTURAL EPSC MEASURES HAVE BEEN INCLUDED IN THE TOTAL PROJECT IMPACTS AND IN THE WATER QUALITY PERMITS.

WETLAND INFORMATION				
TDOT WETLAND LABEL	FROM STATION LT OR RT	TO STATION LT OR RT	TEMPORARY IMPACTS (AC)	PERMANENT IMPACTS (AC)
WTL-1	PIER 6	PIER 8	0	0
WTL-2	PIER 13	END OF BRIDGE	0	0

4.4. TOTAL MAXIMUM DAILY LOADS (TMDL) INFORMATION (1.3.j)

4.4.1. IS THIS PROJECT LOCATED IN A HUC-8 WATERSHED THAT MAINTAINS AN EPA APPROVED TMDL FOR SILTATION AND HABITAT ALTERATION?

☒ YES ☐ NO

4.4.2. IF YES, IS THIS PROJECT LOCATED WITHIN A HUC-12 SUBWATERSHED WITH A WASTE LOAD ALLOCATION (WLA)?

☐ YES ☒ NO

4.4.3. IF YES, DOES THE PROJECT HAVE A DIRECT DISCHARGE TO A 303(d) LISTED STREAM FOR SILTATION?

☐ YES ☐ NO

IF YES, SWPPP INCORPORATES MEASURES OR CONTROLS CONSISTENT WITH THE ASSUMPTIONS AND REQUIREMENTS OF THE TMDL.

4.5. ECOLOGY INFORMATION (3.5.5.e)

DOES THE TDOT ENVIRONMENTAL BOUNDARIES REPORT SPECIFY SPECIAL NOTES TO BE ADDED TO THE PLAN SHEETS?

☐ YES ☒ NO

IF YES, THEY HAVE BEEN INCLUDED ON PLAN SHEET(S) ____.

4.6. ENVIRONMENTAL COMMITMENTS

ARE THERE ANY NOTES ON THE ENVIRONMENTAL COMMITMENT SHEET?

☒ YES ☐ NO

IF YES, THEY HAVE BEEN INCLUDED ON PLAN SHEET(S) 5.

5. **EROSION PREVENTION AND SEDIMENT CONTROL (EPSC) MEASURES (5.5.3.)**

5.1. EPSC MEASURES MUST BE DESIGNED, INSTALLED AND MAINTAINED TO CONTROL STORMWATER VOLUME AND VELOCITY WITHIN THE SITE TO MINIMIZE EROSION (4.1.1).

5.2. EPSC MEASURES MUST CONTROL STORMWATER DISCHARGES, INCLUDING BOTH PEAK FLOWS AND TOTAL STORMWATER VOLUME, TO MINIMIZE EROSION AT OUTLETS, STREAM CHANNELS, AND STREAM BANKS. (4.1.1)

5.3. HAVE THE CONTROL MEASURES BEEN DESIGNED PER THE SIZE AND SLOPE OF THE DISTURBED DRAINAGE AREA (5.5.3.5.)?

☒ YES ☐ NO

5.4. THE CONTROL MEASURES HAVE, AT A MINIMUM, BEEN DESIGNED FOR THE 5-YEAR, 24 HOUR STORM EVENT (5.5.3.5., 6.4.1.b).

5.5. ARE THE LIMITS OF DISTURBANCE CLEARLY MARKED ON THE EPSC PLANS (5.5.1.f)? ☒ YES ☐ NO

5.6. AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.

5.7. UNLESS OTHERWISE NOTED IN THE PLANS, THE CONTRACTOR SHALL NOT CLEAR/DISTURB ANY AREA BEYOND 15 FEET FROM SLOPE LINES OR ROW/ EASEMENT LINE, WHICHEVER IS LESSER.

5.8. CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION, INCLUDING STREAM AND WETLAND BUFFERS (UNLESS PERMITTED), SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.

5.9. HAS A THREE STAGED EPSC PLAN BEEN PREPARED FOR THE PROJECT (5.5.2.)?

YES ☒ NO ☐

PLEASE NOTE THAT A THREE STAGED EPSC PLAN IS REQUIRED FOR ALL TDOT PROJECTS FOR WHICH AN NPDES PERMIT IS REQUIRED.

5.10. STEEP SLOPES ARE DEFINED AS A NATURAL OR CREATED SLOPE OF 35% GRADE OR GREATER REGARDLESS OF HEIGHT. HAVE STEEP SLOPES BEEN MINIMALLY DISTURBED AND/OR PROTECTED BY CONVEYING RUNOFF NON-EROSIVELY AROUND OR OVER THE SLOPE (5.5.3.4.) (10. "STEEP SLOPE")? ☐ YES ☐ NO ☒ N/A

5.11. THE STRUCTURAL EPSC MEASURES HAVE BEEN INCLUDED IN THE TOTAL PROJECT IMPACTS AND HAVE BEEN INCLUDED IN THE AQUATIC RESOURCE ALTERATION (ARAP) PERMIT OR SECTION 401 CERTIFICATION (5.5.1.h). REFER TO THE LIST OF APPLICABLE ENVIRONMENTAL PERMITS LOCATED ON SWPPP SHEET S-7. ALL PERMITS WILL BE MAINTAINED ON SITE WITHIN THE “DOCUMENTATION AND PERMITS” BINDER.

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	43005-4249-04	S-2

- 7.11.2. THE DEPTH OF BORE BELOW THE STREAMBED IS SUFFICIENT TO PREVENT RELEASE OF DRILLING FLUID, BASED ON THE PARENT MATERIAL.
- 7.11.3. A SITE-SPECIFIC CONTINGENCY AND CONTAINMENT PLAN FOR INADVERTENT RELEASE OF DRILLING FLUID SHALL BE ESTABLISHED PRIOR TO COMMENCEMENT OF WORK. THIS PLAN SHALL BE SUBMITTED TO THE TDOT PROJECT ENGINEER AND THE TDOT ENVIRONMENTAL DIVISION PERMITS AND/OR COMPLIANCE AND FIELD SERVICES OFFICE FOR REVIEW AND APPROVAL.

8. MAINTENANCE AND INSPECTION

- 8.1. INSPECTION PRACTICES (5.5.3.9.)

8.1.1. PROJECT EPSC INSPECTORS AND ENGINEERS (INCLUDING TDOT STAFF, CONSULTANTS AND CONTRACTOR STAFF) RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE. AND/OR REPAIR OF EPSC MEASURES SHALL MEET ONE OF THE FOLLOWING REQUIREMENTS (5.5.3.10.):

8.1.1.1. SUCCESSFULLY COMPLETED THE TDOT EPSC INSPECTIONS TRAINING AND ANY RECERTIFICATION COURSE AS REQUIRED.

8.1.1.2. SUCCESSFULLY COMPLETED THE TDEC “LEVEL I - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL” COURSE AND ANY RECERTIFICATION COURSES AS REQUIRED.

8.1.1.3. BE A CURRENT TN LICENSED PROFESSIONAL ENGINEER OR LANDSCAPE ARCHITECT.

8.1.1.4. BE A CURRENT CERTIFIED PROFESSIONAL IN EROSION AND SEDIMENT CONTROL (CPESC).

8.1.1.5. SUCCESSFULLY COMPLETED TDEC "LEVEL II – DESIGN PRINCIPLES FOR EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY RECERTIFICATION COURSE AS REQUIRED.
- 8.1.2. THE TDOT CONSTRUCTION ENGINEER (OR THEIR DULY AUTHORIZED REPRESENTATIVE) AND THE CONTRACTOR'S SITE SUPERINTENDENT ARE RESPONSIBLE FOR INSPECTIONS. MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR. THE TDOT CONSTRUCTION ENGINEER OR THEIR DULY AUTHORIZED REPRESENTATIVE SHALL COMPLETE THE EPSC INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.
- 8.1.3. THE INSPECTOR SHALL CONDUCT PRE-CONSTRUCTION INSPECTIONS TO VERIFY AREAS THAT ARE NOT TO BE DISTURBED HAVE BEEN MARKED IN THE SWPPP AND IN THE FIELD BEFORE LAND DISTURBANCE ACTIVITIES BEGIN AND INITIAL MEASURES HAVE BEEN INSTALLED (10 “INSPECTOR”) (5.5.1.f).
- 8.1.4. EPSC CONTROLS SHALL BE INSPECTED TO VERIFY MEASURES HAVE BEEN INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS, SPECIFICATIONS, AND GOOD ENGINEERING PRACTICES. EPSC INSPECTIONS SHALL BE DOCUMENTED ON THE TDOT EPSC INSPECTION REPORT FORM AND THE TDEC CONSTRUCTION STORMWATER INSPECTION CERTIFICATION (TWICE-WEEKLY INSPECTIONS) FORM.
- 8.1.5. OUTFALL POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING EROSION AND CONTROLLING SEDIMENT INCLUDING SIGNIFICANT IMPACTS TO SURROUNDING STATE WATERS, WOTUS (EPHEMERAL), WETLANDS, OTHER NATURAL RESOURCES AND ADJACENT PROPERTY OWNERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWN GRADIENT LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF-SITE ROADWAY SEDIMENT TRACKING.
- 8.1.6. INSPECTIONS WILL BE CONDUCTED AT LEAST TWICE EVERY CALENDAR WEEK AND AT LEAST 72 HOURS APART (5.5.3.11.a). A CALENDAR WEEK IS DEFINED AS SUNDAY THROUGH SATURDAY. QUALITY ASSURANCE INSPECTIONS OF TDOT EPSC, NPDES AND WATER QUALITY PERMIT REQUIREMENTS SHALL BE PERFORMED PER THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE.

- 8.1.7. THE FREQUENCY OF EPSC INSPECTIONS MAY BE REDUCED TO ONCE A MONTH WHERE SITES OR PORTIONS OF SITES HAVE BEEN TEMPORARILY STABILIZED UNTIL CONSTRUCTION ACTIVITIES RESUME WITH WRITTEN NOTIFICATION BY THE TDOT REGIONAL ENGINEER TO TDEC NASHVILLE CENTRAL OFFICE AND SUBSEQUENT TDEC APPROVAL. WRITTEN NOTIFICATION MUST INCLUDE THE INTENT TO CHANGE FREQUENCY AND JUSTIFICATION (5.5.3.11.a).
- 8.1.8. ALL DISTURBED AREAS OF THE SITE THAT HAVE NOT BEEN PERMANENTLY STABILIZED, AREAS USED FOR MATERIAL STORAGE THAT ARE EXPOSED TO PRECIPITATION, STRUCTURAL CONTROL MEASURES, AND LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE, AND EACH OUTFALL WILL BE INSPECTED (5.5.3.11.b).
- 8.1.9. THE INSPECTOR WILL OVERSEE THE REQUIREMENTS OF OTHER CONSTRUCTION-RELATED WATER QUALITY PERMITS (I.E. TDEC ARAP, USACE SECTION 404, AND TVA SECTION 26a PERMITS) FOR CONSTRUCTION ACTIVITIES AROUND WATERS OF THE STATE (10 “INSPECTOR”).
- 8.1.10. THE SWPPP WILL BE REVISED AS NECESSARY BASED ON THE RESULTS OF THE INSPECTION. REVISION(S) WILL BE RECORDED WITHIN 1 WEEK OF THE INSPECTION. REVISION(S) WILL BE IMPLEMENTED WITHIN 2 WEEKS OF THE INSPECTION (5.5.3.11.e AND 5.5.3.11.f).
- 8.1.11. DOCUMENTATION OF INSPECTIONS WILL BE MAINTAINED ON SITE IN THE “DOCUMENTATION AND PERMITS” BINDER. REPORTS WILL BE SUBMITTED TO THE TDOT PROJECT ENGINEER PER THE CONTRACT.
- 8.1.12. THESE INSPECTION REQUIREMENTS DO NOT APPLY TO DEFINABLE AREAS OF THE SITE THAT HAVE MET PERMANENT STABILIZATION REQUIREMENTS AND HAVE BEEN NOTED IN THE SWPPP.
- 8.1.13. TRAINED CERTIFIED INSPECTORS SHALL COMPLETE INSPECTION TO THE BEST OF THEIR ABILITY. FALSIFYING INSPECTION RECORDS OR OTHER DOCUMENTATION OR FAILURE TO COMPLETE INSPECTION DOCUMENTATION SHALL RESULT IN A VIOLATION OF THIS PERMIT AND ANY OTHER APPLICABLE ACTS OR RULES (5.5.3.11.h).
- 8.2. DULY AUTHORIZED REPRESENTATIVE (8.7.3.)

THE PROJECT ENGINEER MAY DELEGATE AN INDIVIDUAL AND/OR CONSULTANT TO SIGN EPSC INSPECTIONS REPORTS. FOR SATISFYING SIGNATORY REQUIREMENTS FOR EPSC INSPECTION REPORTS, THE PROJECT ENGINEER AND NEWLY AUTHORIZED INDIVIDUAL ACCEPTING RESPONSIBILITY MUST COMPLETE AND SIGN THE TDOT CONSTRUCTION DIVISION EPSC DELEGATION OF AUTHORITY.
- 8.3. MAINTENANCE PRACTICES (5.1 AND 8.13.)

8.3.1. ALL CONTROLS WILL BE MAINTAINED IN GOOD AND EFFECTIVE OPERATING ORDER AND IN ACCORDANCE WITH TDOT STANDARD DRAWINGS AND GOOD ENGINEERING PRACTICES. (5.1. AND 5.5.3.1.b)

8.3.2. MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR.

8.3.3. UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE, MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE 24-HOUR TIMEFRAME, WRITTEN DOCUMENTATION PROVIDED BY THE CONTRACTOR SHALL BE PLACED IN THE FIELD DIARY AND EPSC INSPECTION REPORT. AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION. (5.5.3.11.e).

8.3.4. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES (SEDIMENT TRAPS, SILT FENCE, SEDIMENT BASINS, OTHER CONTROLS, ETC.) WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). (5.5.3.1.d).

8.3.5. DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE STEPS TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE EPSC MEASURES AT THE CONTRACTOR'S OWN EXPENSE.

- 8.3.6. CHECK DAMS WILL BE INSPECTED FOR STABILITY. SEDIMENT WILL BE REMOVED WHEN DEPTH REACHES ONE-HALF (½) THE HEIGHT OF THE DAM.
- 8.3.7. SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS, DOES NOT MIGRATE INTO FEATURES REMOVED FROM, AND DOES NOT MIGRATE ONTO ADJACENT PROPERTIES AND/OR INTO WATERS OF THE STATE/U.S.
- 8.3.8. LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION CHEMICALS EXPOSED TO STORMWATER WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFF THE SITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EROSION CONTROL WILL BE REMOVED (5.5.3.7.a).
- 8.3.9. ALL SEEDED AREAS WILL BE CHECKED FOR BARE SPOTS, EROSION WASHOUTS, AND VIGOROUS GROWTH FREE OF SIGNIFICANT WEED INFESTATIONS.

9. SITE ASSESSMENTS (5.5.3.8.)

QUALITY ASSURANCE SITE ASSESSMENTS OF EROSION PREVENTION AND SEDIMENT CONTROLS SHALL BE PERFORMED PER THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE GUIDELINES.

10. STORMWATER MANAGEMENT (5.5.3.11.h)

- 10.1. STORMWATER MANAGEMENT WILL BE HANDLED BY TEMPORARY CONTROLS OUTLINED IN THIS SWPPP AND ANY PERMANENT CONTROLS NEEDED TO MEET PERMANENT STORMWATER MANAGEMENT NEEDS IN THE POST CONSTRUCTION PERIOD. PERMANENT CONTROLS WILL BE DEPICTED ON THE PLANS AND NOTED AS PERMANENT.
- 10.2. DESCRIBE ANY SPECIFIC POST-CONSTRUCTION MEASURES THAT WILL CONTROL VELOCITY, POLLUTANTS, AND/OR EROSION (5.5.3.6.c): _____
- 10.3. OTHER ITEMS NEEDING CONTROL (5.5.3.7.)

CONSTRUCTION MATERIALS: THE FOLLOWING MATERIALS OR SUBSTANCES ARE EXPECTED TO BE PRESENT ON THE SITE DURING THE CONSTRUCTION PERIOD. (CHECK ALL THAT APPLY).

☒ LUMBER, GUARDRAIL, TRAFFIC CONTROL DEVICES

☒ CONCRETE WASHOUT

☐ PIPE CULVERTS (I.E. CONCRETE, CORRUGATED METAL, HDPE, ETC.)

☐ MINERAL AGGREGATES, ASPHALT

☐ EARTH

☒ LIQUID TRAFFIC STRIPING MATERIALS, PAINT

☒ ROCK

☐ CURING COMPOUND

☐ EXPLOSIVES

☐ OTHER _____

THESE MATERIALS WILL BE HANDLED AS NOTED IN THIS SWPPP.
- 10.4. WASTE MATERIALS (5.5.3.7.c)

WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE WITH THE TDOT CONSTRUCTION CONTRACT AND FEDERAL AND STATE REGULATIONS. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S) CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.
- 10.5. HAZARDOUS WASTE (5.5.3.7.c) (8.8)

ALL HAZARDOUS WASTE MATERIALS WILL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL WILL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S ON-SITE REPRESENTATIVE WILL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR WILL OBTAIN ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.

10.6. SANITARY WASTE (5.5.3.7.b)

PORTABLE SANITARY FACILITIES WILL BE PROVIDED ON ALL CONSTRUCTION SITES. SANITARY WASTE WILL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY LOCAL REGULATIONS. THE CONTRACTOR WILL OBTAIN ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.

10.7. OTHER MATERIALS

THE FOLLOWING MATERIALS OR SUBSTANCES ARE EXPECTED TO BE PRESENT ON THE SITE DURING THE CONSTRUCTION PERIOD. (CHECK ALL THAT APPLY).

- ☐ FERTILIZERS AND LIME
- ☐ PESTICIDES AND/OR HERBICIDES
- ☒ DIESEL AND GASOLINE
- ☒ MACHINERY LUBRICANTS (OIL AND GREASE)

THESE MATERIALS WILL BE HANDLED AS NOTED IN THIS SWPPP.

11. **NON-STORMWATER DISCHARGES (5.5.3.12.)**

11.1. THE FOLLOWING NON-STORMWATER DISCHARGES ARE ANTICIPATED DURING THE CONSTRUCTION OF THIS PROJECT (CHECK ALL THAT APPLY):

- ☒ DEWATERING OF WORK AREAS OF COLLECTED STORMWATER AND GROUND WATER.
- ☒ WATERS USED TO WASH VEHICLES (OF DUST AND SOIL) WHERE DETERGENTS ARE NOT USED AND DETENTION AND/OR FILTERING IS PROVIDED BEFORE THE WATER LEAVES THE SITE.
- ☒ WATER USED TO CONTROL DUST. (3.5.3.1.n)
- ☐ POTABLE WATER SOURCES INCLUDING WATERLINE FLUSHING FROM WHICH CHLORINE HAS BEEN REMOVED TO THE MAXIMUM EXTENT PRACTICABLE.
- ☐ UNCONTAMINATED GROUNDWATER OR SPRING WATER.
- ☒ FOUNDATION OR FOOTING DRAINS WHERE FLOWS ARE NOT CONTAMINATED WITH POLLUTANTS.
- ☐ OTHER: _____

11.2. ALL ALLOWABLE NON-STORMWATER DISCHARGES WILL BE DIRECTED TO STABLE DISCHARGE STRUCTURES PRIOR TO LEAVING THE SITE. FILTERING OR CHEMICAL TREATMENT MAY BE NECESSARY PRIOR TO DISCHARGE. ALL CHEMICAL TREATMENTS MUST BE APPLIED PER SECTION 6 FLOCCULANTS.

11.3. THE DESIGN OF ALL IMPACTED EPSC MEASURES RECEIVING FLOW FROM ALLOWABLE NON-STORMWATER DISCHARGES MUST BE DESIGNED TO HANDLE THE VOLUME OF THE NON-STORMWATER COMPONENT.

11.4. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS WILL NOT BE PERMITTED ON-SITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.

11.5. ARE ANY DISCHARGES ASSOCIATED WITH INDUSTRIAL (NON-CONSTRUCTION STORMWATER) ACTIVITY EXPECTED (5.5.1.g)?

- ☐ YES ☒ NO

IF YES, SPECIFY THE LOCATION OF THE ACTIVITY AND ITS PERMIT NUMBER: _____

12. **SPILL PREVENTION, MANAGEMENT AND NOTIFICATION (5.5.3.7.c, 6.1)**

12.1. SPILL PREVENTION (5.5.3.7.c)

12.1.1. CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ON-SITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE TANKS WITH AGGREGATE STORAGE CAPACITY IN EXCESS OF 1,320 GALLONS SHALL HAVE SECONDARY CONTAINMENT.

12.1.2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN AS REQUIRED BY TDOT SPECIAL PROVISION 107FP (REGARDING WATER QUALITY AND STORM WATER PERMITS) AND THE LAW PRIOR TO STORING 1320 GALLONS ON SITE.

12.1.3. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ON-SITE AND A COPY PROVIDED TO THE TDOT CONSTRUCTION ENGINEER.

12.2. MATERIAL MANAGEMENT

12.2.1. HOUSEKEEPING

ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ON-SITE BY THE CONTRACTOR. EXCEPT FOR BULK MATERIALS THE CONTRACTOR WILL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING WILL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. WHEN POSSIBLE, ALL PRODUCTS WILL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFF SITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS WILL BE FOLLOWED. THE CONTRACTOR'S SITE SUPERINTENDENT WILL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL. DUST GENERATED WILL BE CONTROLLED IN AN ENVIRONMENTALLY SAFE MANNER. VEGETATION AREAS NOT ESSENTIAL TO THE CONSTRUCTION PROJECT WILL BE PRESERVED AND MAINTAINED AS NOTED ON THE PLANS.

12.2.2. HAZARDOUS MATERIALS

PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS UNLESS THE CONTAINER IS NOT RE-SEALABLE. ORIGINAL LABELS AND MATERIAL SAFETY DATA SHEETS WILL BE RETAINED IN A SAFE PLACE TO RELAY IMPORTANT PRODUCT INFORMATION. IF SURPLUS PRODUCT MUST BE DISPOSED OF, MANUFACTURER'S LABEL DIRECTIONS FOR DISPOSAL WILL BE FOLLOWED. MAINTENANCE AND REPAIR OF ALL EQUIPMENT AND VEHICLES INVOLVING OIL CHANGES, HYDRAULIC SYSTEM DRAIN DOWN, DE-GREASING OPERATIONS, FUEL TANK DRAIN DOWN AND REMOVAL, AND OTHER ACTIVITIES WHICH MAY RESULT IN THE ACCIDENTAL RELEASE OF CONTAMINANTS WILL BE CONDUCTED ON AN IMPERVIOUS SURFACE AND UNDER COVER DURING WET WEATHER TO PREVENT THE RELEASE OF CONTAMINANTS ONTO THE GROUND. WHEEL WASH WATER WILL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER WILL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM. POTENTIAL pH-MODIFYING MATERIALS SUCH AS: BULK CEMENT, CEMENT KILN DUST, FLY ASH, NEW CONCRETE WASHINGS AND CURING WATERS, CONCRETE PUMPING, AND MIXER WASHOUT WATERS WILL BE COLLECTED ON SITE AND MANAGED TO PREVENT CONTAMINATION OF STORMWATER RUNOFF.

12.3. PRODUCT SPECIFIC PRACTICES

12.3.1. PETROLEUM PRODUCTS: ALL ON-SITE VEHICLES WILL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE. PETROLEUM PRODUCTS WILL BE STORED IN TIGHTLY SEALED CONTAINERS WHICH ARE CLEARLY LABELED.

12.3.2. FERTILIZERS: FERTILIZERS WILL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED BY TDOT. ONCE APPLIED, FERTILIZERS WILL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER. FERTILIZERS WILL BE STORED IN AN ENCLOSED AREA UNDER COVER. THE CONTENTS OF PARTIALLY USED FERTILIZER BAGS WILL BE TRANSFERRED TO SEALABLE CONTAINERS TO AVOID SPILLS.

12.3.3. PAINTS: ALL CONTAINERS WILL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. THE EXCESS WILL BE DISPOSED OF PER THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.

12.3.4. CONCRETE TRUCKS: CONTRACTORS WILL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED AND NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. UPON COMPLETION OF CONSTRUCTION WASHOUT AREAS WILL BE PROPERLY STABILIZED.

12.4. SPILL MANAGEMENT

IN ADDITION TO THE PREVIOUS HOUSEKEEPING AND MANAGEMENT PRACTICES, THE FOLLOWING PRACTICES WILL BE FOLLOWED FOR SPILL PREVENTION AND CLEANUP IF NECESSARY:

12.4.1. ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTATIVE MAINTENANCE TO REDUCE THE CHANGE OF LEAKAGE AND SPILLS.

12.4.2. FOR ALL HAZARDOUS MATERIALS STORED ON SITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP WILL BE CLEARLY POSTED. SITE PERSONNEL WILL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.

12.4.3. APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT WILL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ON-SITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.

12.4.4. ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA WILL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.

12.4.5. THE CONTRACTOR'S RESPONSIBLE PARTY WILL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.

12.4.6. IF SPILLS REPRESENT AN IMMINENT THREAT OF ESCAPING THE SITE AND ENTERING RECEIVING WATERS, PERSONNEL WILL RESPOND IMMEDIATELY TO CONTAIN THE RELEASE AND NOTIFY THE SUPERINTENDENT AFTER THE SITUATION HAS BEEN STABILIZED.

12.4.7. IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTling PONDS, DETENTION PONDS, SWALES), ACTION WILL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR WILL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.

12.4.8. IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

12.5. SPILL NOTIFICATION (6.1)

WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO, OR MORE THAN A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD:

12.5.1. THE TDOT PROJECT ENGINEER IS RESPONSIBLE FOR NOTIFYING THE REGIONAL PROJECT DEVELOPMENT OFFICE (E.G. TRANSPORTATION ENVIRONMENTAL STUDIES SPECIALIST) AS SOON AS HE OR SHE HAS KNOWLEDGE OF THE DISCHARGE.

12.5.2. THE TDOT REGIONAL PROJECT DEVELOPMENT OFFICE WILL NOTIFY THE LOCAL TDEC ENVIRONMENTAL FIELD OFFICE AND ANY OTHER APPLICABLE REGULATORY AGENCIES WITHIN 24 HOURS OF THE SPILL.

12.5.3. IN ADDITION TO ANY FOLLOW UP NOTIFICATIONS REQUIRED BY FEDERAL LAW, A WRITTEN DESCRIPTION OF THE RELEASE, DATE OF RELEASE AND CIRCUMSTANCES LEADING TO THE RELEASE, WHAT ACTIONS WERE TAKEN TO MITIGATE EFFECTS OF THE RELEASE, AND STEPS TAKEN TO MINIMIZE THE CHANCE OF FUTURE OCCURRENCES WILL BE SUBMITTED TO THE APPROPRIATE TDEC ENVIRONMENTAL FIELD OFFICE WITHIN 2 WEEKS OF KNOWLEDGE OF THE RELEASE.

12.5.4. THE SWPPP MUST BE MODIFIED WITHIN 2 WEEKS OF KNOWLEDGE OF THE RELEASE PROVIDING A DESCRIPTION OF THE RELEASE, CIRCUMSTANCES LEADING TO THE RELEASE, AND THE DATE OF RELEASE. THE SWPPP WILL BE REVIEWED AND MODIFIED AS NECESSARY TO IDENTIFY MEASURES TO PREVENT THE REOCCURRENCE OF SUCH RELEASES AND TO RESPOND TO SUCH RELEASES.

13. RECORD-KEEPING

13.1. REQUIRED RECORDS

TDOT OR THEIR DULY AUTHORIZED REPRESENTATIVE WILL MAINTAIN AT THE SITE THE FOLLOWING RECORDS OF CONSTRUCTION ACTIVITIES (7.2.1.) (7.2.1.):

13.1.1. THE DATES WHEN MAJOR GRADING ACTIVITIES OCCUR.

13.1.2. THE DATES WHEN CONSTRUCTION ACTIVITIES TEMPORARILY OR PERMANENTLY CEASE ON A PORTION OF THE SITE.

13.1.3. THE DATES WHEN STABILIZATION MEASURES ARE INITIATED.

13.1.4. RECORDS EPSC INSPECTION REPORTS AND CORRECTIVE MEASURES.

13.1.5. RECORDS OF QUALITY ASSURANCE SITE ASSESSMENTS.

13.1.6. COPY OF SITE EPSC INSPECTOR'S CERTIFICATION AND/OR LICENSING

13.1.7. A COPY OF ANY REGULATORY CORRESPONDENCE REGARDING THE EFFECTIVENESS OF THE SWPPP OR EPSC CONTROLS.

13.2. RAINFALL MONITORING PLAN (7.2.1.):

13.2.1. EQUIPMENT

AT A MINIMUM, THE CONTRACTOR WILL INSTALL A FENCE POST TYPE RAIN GAUGE TO MEASURE RAINFALL. THE STANDARD FENCE POST RAIN GAUGE WILL BE A WEDGE-SHAPED GAUGE THAT MEASURES UP TO 6 INCHES OF RAINFALL. AN ENGLISH SCALE WILL BE PROVIDED ON ONE FACE, WITH A METRIC SCALE ON THE OTHER FACE. GRADUATION WILL BE PERMANENTLY MOLDED IN DURABLE WEATHER-RESISTANT PLASTIC. THE MINIMUM GRADUATION WILL BE 0.01 INCH (OR 0.1MM). AN ALUMINUM BRACKET WITH SCREWS MAY BE USED TO MOUNT THE GAUGE ON A WOODEN SUPPORT.

13.2.2. LOCATION

THE RAIN GAUGE WILL BE LOCATED AT OR ALONG THE PROJECT SITE, AS DEFINED IN THE NOI OF THE NPDES PERMIT, IN AN OPEN AREA SUCH THAT THE MEASUREMENT WILL NOT BE INFLUENCED BY OUTSIDE FACTORS (I.E. OVERHANGS, GUTTER, TREES, ETC.). AT LEAST ONE RAIN GAUGE PER LINEAR MILE IS REQUIRED ALONG (AS MEASURED ALONG THE CENTERLINE OF THE PRIMARY ALIGNMENT) THE PROJECT WHERE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING IS ACTIVELY PERFORMED, OR EXPOSED SOIL HAS NOT YET BEEN PERMANENTLY STABILIZED.

13.2.3. METHODS

RAINFALL MONITORING WILL BE INITIATED PRIOR TO CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING, OR FILLING, EXCEPT AS SUCH MINIMAL CLEARING MAY BE NECESSARY TO INSTALL A RAIN GAUGE IN AN OPEN AREA. THE RAIN GAUGE WILL BE CHECKED FOR OPERATIONAL SOUNDNESS DAILY (DURING NORMAL BUSINESS HOURS) IN WET TIMES AND WEEKLY IN DRY TIMES. GAUGES WILL BE REPAIRED OR REPLACED ON THE SAME DAY IF FOUND TO BE NON-OPERATIONAL OR MISSING.

13.2.4. EACH RAIN GAUGE WILL BE READ (FOR DETAILED RECORDS OF RAINFALL) AND EMPTIED AFTER EVERY RAINFALL EVENT OCCURRING ON THE PROJECT SITE AT APPROXIMATELY THE SAME TIME OF THE DAY (DURING NORMAL BUSINESS HOURS). DURING PERIODS OF DRY CONDITIONS, IT WILL NOT BE NECESSARY TO READ THE RAIN GAUGE EVERY DAY. IN LIEU OF THIS REQUIREMENT ON WEEKENDS AND ON STATE HOLIDAYS, THE RAIN GAUGES CAN BE EMPTIED THE NEXT BUSINESS DAY AND A REFERENCE SITE USED FOR A RECORD OF DAILY AMOUNT OF PRECIPITATION FOR THOSE DAYS. A REFERENCE SITE IS THE DOCUMENTATION FROM THE CLOSEST GAUGE WITHIN PROXIMITY OF THE PROJECT FROM A RECOGNIZED SOURCE SUCH AS THE NOAA NATIONAL WEATHER SERVICE.

13.2.5. DETAILED RECORDS WILL BE RECORDED OF RAINFALL EVENTS INCLUDE DATES, AMOUNTS OF RAINFALL, AND THE APPROXIMATE DURATION (OR THE STARTING AND ENDING TIMES). THE RAINFALL RECORDS SHALL BE RECORDED ON THE TDOT RAINFALL RECORD SHEET AND SHALL BE MAINTAINED IN THE "DOCUMENTATION AND PERMITS" BINDER.

13.2.6. IF THE RAINFALL EVENT IS STILL IN PROGRESS AT THE DAILY RECORDING TIME, THE GAUGE WILL BE EMPTIED AND THE RECORD WILL INDICATE THAT THE STORM EVENT WAS STILL IN PROGRESS.

13.2.7. RAIN GAUGE INFORMATION (DETAILED RECORDS), INCLUDING THE LOCATION OF THE NEAREST OUTFALL, WILL BE RECORDED ON THE EPSC INSPECTION REPORT FORMS AT THE TIME OF MEASUREMENT.

13.3. KEEPING PLANS CURRENT (5.4.)

13.3.1. THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL REGULATORY OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORMWATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.

13.3.2. THE STAGES DEPICTED WITHIN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL STAGES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION, THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS STAGES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE STAGES OF CONSTRUCTION THAT WILL OCCUR, THUS THESE DOCUMENTS MUST BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.

13.3.3. THE TDOT EPSC INSPECTOR OR THEIR DULY AUTHORIZED REPRESENTATIVE WILL MODIFY AND UPDATE THE SWPPP WHEN ANY OF THE FOLLOWING CONDITIONS APPLY:

13.3.3.1. WHENEVER THERE IS A CHANGE IN THE SCOPE OF THE PROJECT THAT WOULD BE EXPECTED TO HAVE A SIGNIFICANT EFFECT ON THE DISCHARGE OF POLLUTANTS TO THE WATERS OF THE STATE AND WHICH HAS NOT OTHERWISE BEEN ADDRESSED IN THE SWPPP;

13.3.3.2. WHENEVER INSPECTIONS OR INVESTIGATIONS BY SITE OPERATORS, LOCAL, STATE, OR FEDERAL OFFICIALS INDICATE THE SWPPP IS PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANTS FROM CONSTRUCTION ACTIVITY SOURCES, OR IS OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY; WHERE LOCAL, STATE, OR FEDERAL OFFICIALS DETERMINE THAT THE SWPPP IS INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES, A COPY OF ANY CORRESPONDENCE TO THAT EFFECT MUST BE RETAINED IN THE SWPPP;

13.3.3.3. WHEN ANY NEW OPERATOR AND/OR SUB-OPERATOR IS ASSIGNED OR RELIEVED OF THEIR RESPONSIBILITY TO IMPLEMENT A PORTION OF THE SWPPP;

13.3.3.4. TO PREVENT A NEGATIVE IMPACT TO LEGALLY PROTECTED STATE OR FEDERALLY LISTED OR PROPOSED THREATENED OR ENDANGERED AQUATIC FAUNA;

13.3.3.5. WHEN THERE IS A CHANGE IN CHEMICAL TREATMENT METHODS INCLUDING: USE OF DIFFERENT TREATMENT CHEMICALS, DIFFERENT DOSAGE OR APPLICATION RATES OR A DIFFERENT AREA OF APPLICATION NOT SPECIFIED ON THE EPSC PLANS.

13.3.3.6. ALL SWPPP REVISION(S) SHALL BE RECORDED WITHIN 1 WEEK BY THE PROJECT EPSC INSPECTOR.

13.3.3.7. WHEN A TMDL IS DEVELOPED FOR THE RECEIVING WATERS FOR A POLLUTANT OF CONCERN (SILTATION AND/OR HABITAT ALTERATION), CONSTRUCTION SHALL NOTIFY THE PERMITS SECTION FOR PROPER COORDINATION.

13.4. MAKING PLANS ACCESSIBLE

13.4.1. TDOT WILL RETAIN A COPY OF THIS SWPPP (INCLUDING A COPY OF THE "DOCUMENTATION AND PERMITS" BINDER AT THE CONSTRUCTION SITE (OR OTHER LOCATION ACCESSIBLE TO TDEC AND THE PUBLIC) FROM THE DATE CONSTRUCTION COMMENCES TO THE DATE OF PERMANENT STABILIZATION. TDOT WILL HAVE A COPY OF THE SWPPP AVAILABLE AT THE LOCATION WHERE WORK IS OCCURRING ON-SITE FOR THE USE OF OPERATORS AND THOSE IDENTIFIED AS HAVING RESPONSIBILITIES UNDER THE SWPPP WHENEVER THEY ARE ON THE CONSTRUCTION SITE (7.2.).

13.4.2. PRIOR TO THE INITIATION OF LAND DISTURBING ACTIVITIES AND UNTIL THE SITE HAS MET THE PERMANENT STABILIZATION CRITERIA, TDOT OR THEIR DULY AUTHORIZED REPRESENTATIVE WILL POST A NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE WITH THE FOLLOWING INFORMATION (5.3.4.) (7.2.1.):

13.4.2.1. A COPY OF THE NOTICE OF COVERAGE (NOC) WITH THE NPDES PERMIT NUMBER FOR THE PROJECT;

13.4.2.2. THE INDIVIDUAL NAME, COMPANY NAME, E-MAIL ADDRESS (IF APPLICABLE) AND TELEPHONE NUMBER OF THE LOCAL PROJECT SITE OWNER AND OPERATOR CONTACT;

13.4.2.3. A BRIEF DESCRIPTION OF THE PROJECT; AND

13.4.2.4. THE LOCATION OF THE SWPPP.

13.4.3. ALL INFORMATION DESCRIBED IN SECTION 13.4.2 MUST BE MAINTAINED IN LEGIBLE CONDITION. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS INFEASIBLE DUE TO SAFETY CONCERNS, THE NOTICE SHALL BE POSTED IN A LOCAL BUILDING. THE NOTICE MUST BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION WHERE CONSTRUCTION IS ACTIVELY UNDERWAY AND MOVED AS NECESSARY.

13.5. NOTICE OF TERMINATION (9.0.)

13.5.1. WHEN ALL STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES THAT ARE AUTHORIZED BY THE PERMIT ARE ELIMINATED BY PERMANENT STABILIZATION, THE TDOT REGIONAL ENGINEER WILL SUBMIT A NOTICE OF TERMINATION (NOT) THAT IS SIGNED IN ACCORDANCE WITH THE PERMIT TO THE TDEC CENTRAL OFFICE IN NASHVILLE, TN.

13.5.2. FOR THE PURPOSES OF THE CERTIFICATION REQUIRED BY THE NOT, THE ELIMINATION OF STORMWATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY MEANS THE

13.5.2.1. ALL EARTH-DISTURBING ACTIVITIES ON THE SITE ARE COMPLETED AND ALL DISTURBED SOILS AT THE PORTION OF THE CONSTRUCTION SITE WHERE THE OPERATOR HAD CONTROL HAVE BEEN PERMANENTLY STABILIZED; AND

13.5.2.2. ALL CONSTRUCTION MATERIALS, WASTE AND WASTE HANDLING DEVICES, AND ALL EQUIPMENT, AND VEHICLES THAT WERE USED DURING CONSTRUCTION HAVE BEEN REMOVED AND PROPERLY DISPOSED; AND

13.5.2.3. ALL STORMWATER CONTROLS THAT WERE INSTALLED AND MAINTAINED DURING CONSTRUCTION, EXCEPT THOSE THAT ARE INTENDED FOR LONG-TERM USE FOLLOWING TERMINATION OF PERMIT COVERAGE, HAVE BEEN REMOVED; AND

13.5.2.4. ALL POTENTIAL POLLUTANTS AND POLLUTANT GENERATING ACTIVITIES ASSOCIATED WITH CONSTRUCTION HAVE BEEN REMOVED; AND

13.5.2.5. THE PERMITTEE HAS IDENTIFIED WHO IS RESPONSIBLE FOR ONGOING MAINTENANCE OF ANY STORMWATER CONTROLS LEFT ON THE SITE FOR LONG-TERM USE FOLLOWING TERMINATION OF PERMIT COVERAGE; AND

13.5.2.6. TEMPORARY EPSC MEASURES HAVE BEEN OR WILL BE REMOVED AT AN APPROPRIATE TIME TO ENSURE PERMANENT STABILIZATION IS MAINTAINED; AND


13.5.2.7. ALL STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES FROM THE IDENTIFIED SITE THAT ARE AUTHORIZED BY A NPDES GENERAL PERMIT HAVE OTHERWISE BEEN ELIMINATED FROM THE PORTION OF THE CONSTRUCTION SITE WHERE THE OPERATOR HAD CONTROL.

13.6. RETENTION OF RECORDS (7.1.)

TDOT WILL RETAIN COPIES OF THE SWPPP, ALL REPORTS REQUIRED BY THE PERMIT, AND RECORDS OF ALL DATA USED TO COMPLETE THE NOTICE OF INTENT FOR THE PROJECT FOR A PERIOD OF AT LEAST THREE (3) YEARS FROM THE DATE THE NOT WAS FILED.

14. SITE WIDE/PRIMARY PERMITTEE CERTIFICATION (8.7.5.)

I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED BY ME, OR UNDER MY DIRECTION OR SUPERVISION. THE SUBMITTED INFORMATION IS TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE, AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT. AS SPECIFIED IN TENNESSEE CODE ANNOTATED SECTION 39-16-702(a)(4), THIS DECLARATION IS MADE UNDER PENALTY OF PERJURY.



Digitally signed by Anthony Myers
Date: 2024.05.23 13:06:30 -05'00'

AUTHORIZED TDOT PERSONNEL SIGNATURE (5.3.3.)

Anthony Myers

PRINTED NAME

TDOT Manager

TITLE

05/23/2024

DATE

15. SECONDARY PERMITTEE (OPERATOR) CERTIFICATION (8.7.6.)

I CERTIFY UNDER PENALTY OF LAW THAT I HAVE REVIEWED THIS DOCUMENT, ANY ATTACHMENTS, AND THE SWPPP REFERENCED ABOVE. BASED ON MY INQUIRY OF THE CONSTRUCTION SITE OWNER/DEVELOPER IDENTIFIED ABOVE AND/OR MY INQUIRY OF THE PERSON DIRECTLY RESPONSIBLE FOR ASSEMBLING THIS NOI AND SWPPP, I BELIEVE THE INFORMATION SUBMITTED IS ACCURATE. I AM AWARE THAT THIS NOI, IF APPROVED, MAKES THE ABOVE-DESCRIBED CONSTRUCTION ACTIVITY SUBJECT TO NPDES PERMIT NUMBER TNR1000000, AND THAT CERTAIN OF MY ACTIVITIES ONSITE ARE THEREBY REGULATED. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS, AND FOR FAILURE TO COMPLY WITH THESE PERMIT REQUIREMENTS. AS SPECIFIED IN TENNESSEE CODE ANNOTATED SECTION 39-16-702(a)(4), THIS DECLARATION IS MADE UNDER PENALTY OF PERJURY.

AUTHORIZED CONTRACTOR PERSONNEL SIGNATURE (5.3.3.)

PRINTED NAME

TITLE

DATE

16. ENVIRONMENTAL PERMITS (1.5.2.)

LIST ALL ENVIRONMENTAL PERMITS AND EXPIRATION DATES FOR PROJECT (TO BE COMPLETED AT THE ENVIRONMENTAL PRECONSTRUCTION MEETING BY TDOT CONSTRUCTION OR THEIR DULY AUTHORIZED REPRESENTATIVE):

ENVIRONMENTAL PERMITS			
PERMIT	YES OR NO	PERMIT OR TRACKING NO.	EXPIRATION DATE*
TDEC ARAP	YES		
CORPS OF ENGINEERS (USACE)	YES		
TVA 26A	NO		
TDEC CGP	YES		
OTHER:			

*THE TDOT ENVIRONMENTAL DIVISION MUST BE NOTIFIED SIX MONTHS PRIOR TO PERMIT EXPIRATION DATE.

